

May 28, 2025

Mayor and Council

Village of Anmore
2697 Sunnyside Road
Anmore, BC V3H 5G9

Dear Mayor + Council,

Re: Revised Anmore South Neighbourhood Plan Submission

On behalf of icona Properties and Placemark Design Studio we would like to thank Council for the thoughtful and constructive direction provided during last night's meeting. We acknowledge and appreciate the resolution passed to refer the Official Community Plan amendment application back to staff and the applicant to explore a significant reduction in the number of condominiums and an increase in ground-oriented homes.

Over the last four years, and through more than 28 community meetings, we have engaged extensively with residents of Anmore. It has become clear through this deep and sustained engagement that while many community members support the key elements of the neighbourhood plan, including its parks, trails, community centre, and protected natural areas, there is a broad sentiment that the initial number of condominium units was simply too high.

It is worth noting that our original application proposed approximately 3,500 residential units. Through ongoing community dialogue and, more recently the revised application now reflects half that number. This is a plan shaped not in isolation, but through a process of listening. We believe the result is a balanced and responsive proposal that aligns more closely with Anmore's values.

We were also encouraged to see that, in the recent community survey conducted by the Village, a majority of residents identified housing diversity as a priority for Anmore's future. This confirms the value of introducing a range of housing types that can accommodate evolving needs while respecting the community's rural character.



Understanding the importance of timeliness, our team worked through the night and throughout the day today to respond to Council's resolution. Enclosed with this letter, you will find a revised version of the Anmore South Neighbourhood Plan that:

- Reduces the number of apartment units by 50% – from 1,302 to 650;
- Reduces the number of standalone apartment buildings from 22 to 10, primarily concentrated around the mixed-use commercial centre and in later development phases;
- Increases the number of townhouse units by 25% – from 760 to 960, providing more ground-oriented housing suitable for families;
- Retains all parks, trails, conservation + recreation lands, community centre, and civic reserves previously proposed;
- Maintains previously proposed network of safe, liveable streets along with sufficient residential density to support improved transit service; and,
- Provides regional trunk sewer and water connection to the Anmore South.

We have also provided an updated property tax analysis to reflect the revised land use and population assumptions. The analysis demonstrates that by reducing the total number of residential units by 452, the projected population will decrease accordingly from 4,500 to 3,911, resulting in proportionate reductions in anticipated municipal service costs. These reductions are particularly notable in services calculated on a per-unit or per-capita basis, such as fire and police.

We hope that by delivering this revised submission promptly, it will support the scheduling of an updated OCP amendment for consideration at the June 10th Council meeting.

Thank you again for your continued review and consideration of this application.

Sincerely,



Greg Moore
CEO, icona Properties



Theo Finseth
Partner, Placemark Design Studio

Attachments:

- Revised Anmore South Neighbourhood Plan
- Updated Fiscal Impact Analysis





Anmore South
NEIGHBOURHOOD PLAN

ACKNOWLEDGMENT OF INDIGENOUS TERRITORY

The Village of Anmore acknowledges the many Indigenous peoples of our area, including the following First Nations with communities that are located in the region that we now know as Metro Vancouver: Musqueam, Squamish, Tsleil-Waututh, and Kwikwetlem.

Further, we would like to honour the important place in history occupied by the many territorial keepers of these lands and waters around us since time immemorial.

Anmore South NEIGHBOURHOOD PLAN

PREPARED FOR:



PREPARED BY:



TECHNICAL TEAM:



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APPENDIX A: Geotechnical Investigation Report

GeoPacific Consultants Ltd, November 2023

APPENDIX B: Archaeological Impact Assessment

Inlailawatash Limited Partnership, April 2024

APPENDIX C: Environmental Impact Assessment

AquaTerra Environmental Consultants, March 2025

APPENDIX D: Visual Impact Assessment

Placemark Design + Development, March 2025

APPENDIX E: Retail Market Analysis

City Squared Consulting, February 2025

APPENDIX F: Transportation Impact Assessment

Bunt & Associates, March 2025

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APPENDIX H: Fiscal Impact Analysis

Deloitte, April 2025

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Deloitte, March 2025

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Overview + Context

1. OVERVIEW + CONTEXT

1.1 ANMORE SOUTH CONTEXT

The Village of Anmore is a picturesque community located north of Burrard Inlet, approximately 8 km from downtown Port Moody, spanning 2,714 ha (6,706 ac) with a population of approximately 2,450 residents.

Situated at the southwest corner of the Village and adjacent the regional Urban Containment Boundary, the 61 ha (151 ac) Anmore South Neighbourhood Plan area is bordered by existing neighbourhoods and Anmore Elementary School to the east, existing neighbourhoods to the north, and the City of Port Moody boundary to the west and south (Refer to Figure 1: Context Plan).

Anmore South has a dramatic topographic relief, sloping generally to the southwest with views towards Burnaby Mountain. The lands sit at the lowest elevation within Anmore, well below the Village's surrounding neighbourhoods. Despite being actively logged in the early 1900s, the land today is characterized by 3rd-growth forest with the defining features of Doctor's Creek and Schoolhouse Creek.

1.2 REGIONAL CONTEXT

With a population of over 3 million, Metro Vancouver is the third-largest metropolitan centre in Canada and the social, economic and cultural core of British Columbia. With continued population growth, the Metro Vancouver region faces significant challenges, from housing affordability and social equity to environmental preservation and economic resilience. As one of the 21 member municipalities of Metro Vancouver, Anmore has a role to play in a sustainable future for the region. One of the most significant challenges facing Anmore is the need to accommodate growth and development in a manner that preserves the community's natural environment and character.

Metro Vancouver's 2050 Regional Growth Strategy (Metro 2050) articulates a collective vision for how growth will be managed to support the creation of complete, connected and resilient communities. The Regional Context Statement in Anmore's Official Community Plan (OCP) describes how the Village intends to address the goals of Metro 2050. As an alternative to single-family infill development throughout all of Anmore, the Anmore South Neighbourhood Plan enables the Village to strategically accommodate change in a way that meets regional growth objectives.

1.3 SPECIAL STUDY AREA

The Anmore South Neighbourhood Plan area (formerly IOCO Lands) is designated for major future development within the Official Community Plan (2014). The position, scale, and designation of Anmore South provide an opportunity to accommodate future growth and allow for a more complete community within Anmore.

Historically identified in the Greater Vancouver Regional District's 1996 Livable Region Strategic Plan as a growth concentration area, Anmore South continued to be identified in Metro 2040 and Metro 2050 as a Special Study Area. The Special Study Area designation recognizes the need for preparation of a Neighbourhood Plan to determine land use and housing form, environmental protection, servicing, transportation, community facilities, parkland and financial contributions.

Under Metro 2050 Policy 6.10, the Special Study Area identifies the intention to alter the future land use of Anmore South. Pursuant to this policy, the Anmore South neighbourhood is intended for inclusion within the Urban Containment Boundary and Sewerage + Drainage District, providing for a more complete mix of neighbourhood uses.

1.4 RATIONALE FOR THE NEIGHBOURHOOD PLAN

Adopted as part of the OCP, Neighbourhood Plans provide detailed guidance for new development within a given geographic area, structuring land use and servicing to effectively manage growth towards building a more healthy and complete community.

The Anmore South Neighbourhood Plan (the Neighbourhood Plan) was undertaken to realize the vision of a walkable, mixed-use neighbourhood with a diversity of homes, shops and services within Anmore. Following best practices in community design, the Neighbourhood Plan was prepared in conjunction with a public planning process and extensive Village-led community consultation following a Council-approved Neighbourhood Plan Terms of Reference.

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1.5 FRAMEWORK FOR MANAGING CHANGE

Setting the framework for the phased development of a mixed-use neighbourhood, the Neighbourhood Plan provides the detail required to assist in the reconciliation of land use, environment, density, infrastructure and implementation issues, serving as a comprehensive guide for managing future rezoning, subdivision and development permit applications.

To ensure the long-term build-out of Anmore South is in accordance with the overarching community vision, the Neighbourhood Plan includes Land Use descriptions, schedules and policies, infrastructure servicing concepts and schedules, implementation policies and Development Permit Guidelines.

1.6 ALIGNMENT WITH METRO 2050 REGIONAL GROWTH STRATEGY

The Metro 2050 Regional Growth Strategy structures the actions needed to address identified regional challenges. Outlining the region's shared vision of how projected population, housing, and job growth will be managed over the next 30 years, the strategies and actions of Metro 2050 are structured around five goals:

1. Create a Compact Urban Area;
2. Support a Sustainable Economy;
3. Protect the Environment, Address Climate Change, and Respond to Natural Hazards;
4. Provide Diverse and Affordable Housing Choices; and,
5. Support Sustainable Transportation Choices.

As Anmore's only Neighbourhood Plan area within the region's Urban Containment Boundary, Anmore South is the Village's strategic opportunity to accommodate growth in a way that supports the regional planning objectives. The Regional Context Statement in the Village OCP summarizes the specific Neighbourhood Plan policies that address and support the Metro 2050 goals.

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Neighbourhood Vision

2. NEIGHBOURHOOD VISION

2.1 VISION FOR THE NEIGHBOURHOOD

The vision for Anmore South is one of a complete mixed-use community – celebrating the Village’s natural character and seamlessly integrating a diversity of homes into the surrounding environment. It is a vision that sensitively manages growth, fostering long-term prosperity, while allowing for a more complete community within Anmore.

The Neighbourhood Plan envisions Anmore South as structured according to walkable neighbourhoods, linked through a network of active parks and forest trails – all supported by new recreational amenities and civic infrastructure.

Marking Anmore’s western gateway, Anmore South proposes a socially diverse neighbourhood. Offering a full range of housing forms, it will accommodate up to 1,750 homes with a projected population of approximately 3,900 residents. Compact neighbourhoods are interwoven with an active transportation network of liveable streets, community greenways, and accessible trails – all within a 5-minute walk of each home.

To be more self-sustaining, Anmore South is envisaged with a vibrant neighbourhood heart including local grocery, shops and services, a community centre, and market square – providing local employment and allowing Anmore’s retail spending to stay in the community.

Protecting what’s important, the Neighbourhood Plan seeks to dedicate ~42% (25.6 ha | 63.4 ac) of the land for public use as neighbourhood parks, greenways, and natural areas to ensure the land is protected for future generations.

The Neighbourhood Plan vision will allow Anmore South to be more compact, socially supportive, and land efficient. This land use efficiency serves to protect the environment, reduce greenhouse gas emissions, increase infrastructure efficiency, and foster healthy, diverse communities.

2.2 PLANNING PRINCIPLES

The Neighbourhood Plan reflects sustainable planning that balances the ecological, social and economic aspects of a community by utilizing the following planning principles to help guide the vision:

Ecological | WORKING WITH NATURE

Conserve Ecological Integrity

- Identify and protect both significant and sensitive terrestrial and aquatic habitats.
- Design an interconnected network of wildlife corridors to secure habitat and ensure functional ecology at a landscape scale.
- Minimize future disturbance of natural systems through comprehensive master-planning.
- Promote stewardship of natural systems through interpretive programs and outdoor educational opportunities in cooperation with local stewardship groups.

Create Networks of Parks + Natural Space

- Provide public parks and natural spaces that are accessible within a 5-minute walking distance of each home.
- Link neighbourhoods and natural areas with a pedestrian pathway network.
- Program active and passive community parks for gatherings and recreation.
- Create a variety of parks and natural spaces to accommodate different activities and age groups.

Celebrate Our Natural Heritage

- Promote active education and appreciation of west coast natural history.
- Recognize local history and culture in public realm design.
- Integrate community uses into the landscape, celebrating the unique views and vistas, landforms and natural character.
- Nurture community history and memorialize local people, places and events.
- Design public spaces that reflect the sense of place through use of native plants and local materials.

Social | LIVING LIGHTLY

Foster a Vibrant + Diverse Community

- Accommodate a range of lifestyles and life-stages.
- Provide a variety of housing choices, from compact single family and duplexes to row-homes and village apartments.
- Plan for “aging in place” through a variety of housing choices and encouraging the “Safer Home” standards or similar guidelines in response to a maturing population.
- Promote a variety of housing tenures from fee simple and strata ownership to purpose-built rental and affordable, non-market rental).
- Provide a range of amenities for all age groups throughout the neighbourhood.
- Provide more affordable housing options for first-time buyers.

Design Compact, Walkable Neighbourhoods

- Create a sense of place within each neighbourhood.
- Provide a coherent neighbourhood pattern of streets + pathways with a variety of home + lot sizes.
- Design streets for people and create an enjoyable pedestrian environment.
- Encourage walking through the incorporation of amenities + parks within a 5-minute walking distance of each home.
- Promote neighbourhood safety by designing homes that address the public realm with “eyes on the street.”

Celebrate Local Art + Culture

- Incorporate a sense of place in the design of the public realm.
- Design opportunities to live, work and play in the community.
- Interpret and highlight local cultural history for residents and visitors.
- Plan for the integration of art, theatre, and other local cultural activities into programming of gathering spaces.
- Infuse local art and culture into the public realm, especially streets, parks, plazas, to celebrate civic life.

Economic | SUPPORTING LOCAL LIFESTYLES

Support a Financially Sound Community

- Create a walkable mixed-use centre serving as a focal point for employment, shopping, and social gathering.
- Incorporate residential density to support local businesses and community facilities.
- Diversify municipal revenue sources so support fiscally sustainable local government.
- Plan compact neighbourhoods to reduce servicing networks and maintenance costs.
- Establish a mix of land uses, household types and building forms for a variety of residents.

Plan for Alternative Transportation

- Accommodate all modes of transport, especially walking, cycling and public transit.
- Provide dedicated neighbourhood bike and pedestrian pathways to link community destinations.
- Establish a transit-friendly street network, with future shuttles to transit centres.
- Reduce vehicle trips by providing local neighbourhood shops + services close to home.
- Explore future alternatives, such as community cars, to reduce auto-dependence.

Employ Green Infrastructure

- Foster local food systems through markets, community gardening and outdoor educational opportunities.
- Implement innovative best practices for rainwater and stormwater management.
- Explore alternative energy solutions, such as geo-exchange and passive solar design.
- Design greener streets that minimize the visual, environmental and physical impacts from conventional roads.
- Promote native plantings in landscape design.

2.3 NEIGHBOURHOOD CHARACTER

The character of Anmore South will be established through the relationship between the neighbourhood's built form and its surrounding natural landscape.

The Neighbourhood Plan identifies the following components which will define and preserve the character of Anmore.

Diversity of Homes

A diverse mix of home types, tenures and sizes with an emphasis on expanding the range of affordability and choice, while respecting the character of adjacent neighbourhoods. More urban, compact housing forms better support the overall design intent of the Neighbourhood Plan as a more walkable and complete community that protects a greater proportion of the natural landscape.

Green Network

A significant portion of the Neighbourhood is to be retained in its existing natural state. The Green Network will foster Anmore's character through the protection of defining landscapes, provision of active and passive recreation opportunities, and conservation of functional ecosystem habitat.

Livable Streets

Safe neighbourhood streets designed with pedestrian and cycle pathways that fit the land, complement the system of parks and trails, and provide alternative pedestrian-oriented routes through the community.

Walkable Scale

Built form and orientation of homes within a walkable neighbourhood designed in response to the land, views and solar aspect.

Public Realm

A strong public realm that includes tree-lined, pedestrian oriented streets, a commercial mixed-use village plaza for social gathering and a public community centre that serves as the heart of neighbourhood recreation.

West Coast Landscape

Landscape design that serves to enrich and unify architectural elements and public realm spaces, through the use of west coast materials, forms, colours and native plants that celebrate Anmore's sense of place.

2.4 NEIGHBOURHOOD OUTCOMES

The Neighbourhood Plan realizes a viable community by achieving key performance metrics for community sustainability. Key Neighbourhood Metrics to inform decision making through implementation include the following:

Urban Density

- Average residential density of 11.6 units per acre (upa) or 28.6 units per hectare (uph) within the gross area.
- Average residential density of 28.2 units per acre (upa) or 69.7 units per hectare (uph) within the lands designated for residential development.
- Projected population of ~3,900.

Housing

- 1,750 projected homes, with a unit mix composed of 20 single-family homes, 120 duplexes, 960 townhomes and 650 apartments.
- Actual unit mix by housing type may be adjusted provided the overall unit number is not exceeded.

Civic

- A Civic Community Centre on a 0.49 ha (1.21 ac) site, providing for a ~16,000 sq ft facility.
- 2 Civic Development Reserves totaling 0.48 ha (1.19 ac) to provide the opportunity for additional civic use.

Commercial

- 2 Mixed-Use sites totaling 1.50 ha (3.70 ac) with the opportunity for ground floor commercial use, anticipating ~41,000 - 50,000 sq ft of commercial floorspace, and ~8,000 sq ft of professional office space.

Green Network

- Conservation + Recreation lands totaling 21.35 ha (52.76 ac) of retaining existing forest, representing 35% of the Plan area.
- Neighbourhood Parks totaling 4.34 ha (10.72 ac) of public open space distributed within a 5-minute walk of each home.
- Dedicated off-street trails, multi-use paths and greenways totaling ~5km.

Transportation Network

- Safe Livable Streets with new Collector and Local Street standards
- 8 Neighbourhood Street accesses connecting to the existing Village street network.
- Pedestrian and Cycling Network including Village Greenways and Community Pathways through Conservation + Recreation lands, including connection to Anmore Elementary School and Park.

Servicing

- New municipal infrastructure systems as identified in Schedule G: Water Servicing Concept, Schedule H: Sanitary Servicing Concept, and Schedule I: Drainage Concept.

3



Biophysical Findings

3. BIOPHYSICAL FINDINGS

The biophysical findings provide a detailed understanding of Anmore South's complexities and provide the foundation for technical planning and neighbourhood design.

3.1 HISTORICAL SETTING

Anmore South is located within the Northwest Coast Culture Area as defined by anthropologists, an immense area that encompasses the west coast of North America from southern Alaska to northern California. Coast Salish First Nations, including the Musqueam, Squamish, Stó:lō, Kwikwetlem, and Tsleil-Waututh, have lived around Burrard Inlet since time immemorial, with the earliest identified cultural artifacts dating back approximately 10,000 years. The lands continue to be the traditional territory of local First Nations.

The Anmore South lands were first logged by European settlers in the early 20th century. The property was a portion of the Ioco lands through most of the 20th century and at one time was considered as the potential domestic water source for the Ioco Townsite.

Now within the municipality of Anmore, the Anmore South neighbourhood has been identified as a major future development area since the Village's 2014 OCP.

3.2 PHYSICAL + TOPOGRAPHIC ANALYSIS

Characterized by complex landforms with deep ravines and forested hillsides, the Neighbourhood Plan offers dramatic southwest views that reflect a distinct landscape within the Village. The biophysical analysis of the Neighbourhood Plan was completed using 2024 LiDAR data, providing a detailed understanding of the land's topographic complexities, and provides the foundation for the technical studies undertaken.

Landform | Hillside basin

The Neighbourhood Plan spans a significant range in elevation - from 15m in the southwest near First Avenue to 165 m in the northeast near Fern Drive. The topographic range results in 3 'neighbourhood levels' - a lower level below Sunnyside Road; a middle level forming the neighbourhood's heart; and an upper level adjacent to Anmore Elementary School. (Refer to Figure 3: Landform Analysis)

Slope + Geotech | Incised hillside ravines

The Neighbourhood Plan area has a consistent hillside topography of gentle to moderate slopes, with areas of steep slopes (>30%) generally related to the Schoolhouse Creek and Doctor's Creek ravines systems. The hillside quality of the land requires neighbourhood planning and design that works with the topography. In contrast to the Neighbourhood Plan areas northern portions, extensive gentle areas south of Sunnyside Road provide opportunities for programmed community recreation. (Refer to Figure 4: Slope Analysis)

Aspect + Views | Sunny southwest views

The Neighbourhood Plan area's dominant southern and south-western aspect takes advantage of afternoon and evening sunlight, including opportunities for positive solar gain through building siting. Higher elevation portions of the site offer views of Burnaby Mountain and Burrard Inlet (Refer to Figure 5: Aspect Analysis).

3.3 GEOTECHNICAL FINDINGS

Geotechnical Investigation Report

November 2023

GeoPacific Consultants Ltd.

The Geotechnical Assessment Report presents a desktop and field assessment that identifies anticipated conditions for Anmore South. Refer to Appendix A: Geotechnical Investigation Report for further details.

Soil Conditions

Based on Geologic Survey of Canada information, the site is described as underlain by Capilano Sediments and Vashon Drift glacial till, with post glacial soils expected to be mostly present at the southwestern portion of the site. Capilano Sediments are described as raised marine beach, spit, bar and lag veneer, poorly sorted sand to gravel normally less than 1m thick but up to 8 m thick. Vashon Drift generally consists of lodgment and minor flow till, lenses and interbeds of glaciolacustrine laminated stony silt. Soil conditions noted at test hole locations were generally consistent with published geology for the area.

Groundwater Conditions

According to the BC Water Resources Atlas, the southeast portion of the site is within the mapped extent of Aquifer #924, which is comprised of confined glaciofluvial sand and gravel underneath glacial till. The static groundwater level recorded at the nearest registered well – WTN 74082, directly adjacent to the southeast corner of the site – is approximately 27.4 m below ground surface. Some perched groundwater may form at the contact with the upper weathered soils and the relatively impermeable glacial till in sandier zones with glacial till. The main recharge mechanism for perched groundwater of this nature is the percolation of precipitation. GeoPacific installed four groundwater monitoring wells on-site to further understanding of the perched groundwater conditions.

Geotechnical Recommendations

The Geotechnical Investigation Report identifies the following required field reviews for development of the Anmore South lands as part of detailed design:

- Review of site stripping;
- Review of foundation subgrade prior to footing construction;
- Review of slab-on-grade fill compaction prior to slab construction;
- Review of the compaction of engineered fill;
- Review of any temporary cut slopes or excavation in excess of 1.2 m in height prior to worker entry;
- Review of pavement structure subgrade prior to sub-base placement; and,
- Review of base and sub-base fill materials and compaction.

3.4 ARCHAEOLOGICAL FINDINGS

Archaeological investigation for Anmore South consisted of a Site Records Request to the Provincial Archaeology Branch and an Archaeological Impact Assessment conducted under applicable provincial permits.

Archaeological Site Records Request

February 2023

Archaeology Branch | Ministry of Forests

The Provincial Archaeological Inventory Search identified that there are no known archaeological sites recorded on the Anmore South lands and that there is a high potential for previously unidentified archaeological sites to exist on the site, based on the underdeveloped nature of the properties and lack of inland surveying.

Archaeological Impact Assessment (AIA) Report

April 2024

Inlailawatash Limited Partnership

Inlailawatash applied for and received heritage permits from Musqueam, Squamish, Stó:lō, Kwikwetlem, and Tsleil-Waututh Nations for an Archaeological Impact Assessment (AIA) on the Anmore South lands.

Results of the initial desktop study indicated potential for archaeological materials to be present in the Project Area. This is supported by the presence and abundance of known archaeological sites nearby. A field survey included walked transects and ground observations covering 92% of the site. The field crew excavated 13 shovel tests across three micro-landforms identified during the survey. All shovel tests were negative for archaeological materials.

Archaeological Recommendations

The AIA determined that no further archaeological investigations or monitoring are required for the Project Area.

As low-density archaeological sites may remain undetected in the Project Area, Chance Find Procedures (CFP) will be implemented during all ground disturbing activities associated with future development of Anmore South, in accordance with recommendations of the AIA.

Refer to Appendix B: Archaeological Impact Assessment for further details.

3.5 ENVIRONMENTAL FINDINGS

Environmental Impact Assessment

March 2025

AquaTerra Environmental Consultants

The Environmental Impact Assessment provides a summary of environmental fieldwork and science-based recommendations for environmental planning objectives to guide the environmentally responsible development planning for Anmore South. Refer to Appendix C: Environmental Impact Assessment for further details.

Fish + Aquatic Habitat

A total of twelve watercourses were identified within the Anmore Lands site boundaries, north of Sunnyside Road: the Doctor's Creek main stem and three associated tributaries; Schoolhouse Creek North and nine associated tributaries; and the drainage ditch on the north side of Sunnyside Road. Watercourses on the north side of Sunnyside Road were classified as non-fish bearing. Fish bearing watercourses on the south side of Sunnyside Road included Doctor's Creek and Doctor's Tributary 1, Schoolhouse Creek North, Schoolhouse Tributary 5, and Schoolhouse Tributary 3. Fish species observed in these watercourses include Coho Salmon, Chum Salmon, and Cutthroat Trout. Preliminary watercourse development setbacks were identified and Detailed Riparian Area Protection Regulation Assessments are required to determine the specific regulatory setback prior to any development on the lands.

Wildlife + Terrestrial Habitats

The site is dominated by mature second-growth coniferous forest with pockets of mature mixed forest, deciduous woods and regenerating forest. A variety of common mammal species were observed directly or indirectly including Douglas Squirrel and Blacktailed Deer, Raccoon, Coyote and Black-bear. Observations of birds included American Robin, Bald Eagle, Black-capped Chickadee, Chestnut-backed Chickadee, Common Raven, Dark-eyed Junco, Downy Woodpecker, Golden-crowned Kinglet, Hairy Woodpecker, Northwestern Crow, Northern Flicker, Pacific Wren, Song Sparrow, and Spotted Towhee. Amphibian observations were limited to Northwestern Salamander located in Schoolhouse Creek, but habitats within the site area may be utilized by a variety of common amphibian and reptile species including Pacific Tree Frog, Long-toed Salamander, Ensatina, Common Garter Snake, and Northwestern Garter Snake.

Species at Risk

Observed or reported species-at-risk included Coastal Cutthroat Trout (provincially blue-listed) and Coastal Tailed Frog (SARA Schedule 1 and provincially blue-listed) within Schoolhouse Creek and some tributaries. Northern Red-legged Frog (SARA Schedule 1 and provincially blue-listed) have been observed within Mossom Creek to the northeast of the site. Additionally, although Anmore South is outside the Mossom Creek Watershed, an unconfirmed record of Pacific Water Shrew (SARA Schedule 1 and provincially red-listed) has been reported off-site within the Mossom Creek corridor.

Environmental Recommendations

The Environmental Impact Assessment identifies the following recommendations to be implemented during neighbourhood development to ensure that environmental impacts are avoided or appropriately mitigated:

- Implement and adhere to a site-specific Construction Environmental Management Plan (CEMP). This will include sub-plans including, but not limited to, a Sensitive Species Management Plan, Emergency Spill Response Plan and Erosion Control Plan.
- Habitat restoration or offsetting, where loss has been identified.
- Design and implementation of an Invasive Species Management Plan.
- Wildlife-friendly designs for stream crossings and riparian zone buffers.
- Monitoring sensitive species and ensuring compliance with environmental regulations (e.g., Riparian Areas Protection Regulation).

3.6 VISUAL IMPACT ASSESSMENT

Visual Impact Assessment

March 2025

Placemark Design + Development

Building on the mapped environmental and community values, the Visual Impact Assessment documents the efficacy of the neighbourhood's retrained forest buffers in preventing visual impacts of Anmore South on existing neighbourhoods.

Using LiDAR data, the terrain and existing tree canopy of the Neighbourhood Plan were modeled using Infracore 3D modeling software to generate an accurate representation of existing conditions.

Owing to the neighbourhood's downslope position and the effectiveness of the retained forest buffers, the 3D Visual Impact Assessment demonstrated that Anmore South's development would not adversely impact views from existing Anmore neighbourhoods.

The Anmore South Neighbourhood Plan minimizes visual impacts on the existing natural landscape through the following mitigations:

- **Conservation Framework** | Dedicates 35% (21.3 ha | 52.7 ac) of Anmore South as Conservation + Recreation lands to preserve the forested character and mitigate the scale of visual impact.
- **Forest Buffer** | Retains a minimum 15 m | 50 ft forest buffer along Sunnyside Road to maintain Anmore's forested arrival sequence.
- **Natural Interface** | Retains a 15 m | 50 ft forest buffer between future development and adjacent residential neighbourhoods.
- **Natural Topography** | Aligns future development and infrastructure with existing landforms and topography to reduce extent of clearing and grading.
- **Clustered Development** | Limits future development to defined areas to reduce clearing and ensure natural landscape connections.
- **Building Heights** | Limits future apartment buildings to six storeys to minimize visual impact above the forest ridgeline.
- **Native Screening** | Employs native plant palette within public spaces and streetscapes to establish new habitat and screen the appearance of buildings.
- **Street Trees** | Incorporates street tree within new public boulevards to provide shade, canopy habitat, pedestrian comfort and safety.
- **Minimize Light Pollution** | Adopts Dark Sky standards with public street lighting that uses shielded and downward-facing lights to reduce glare and preserve night skies.

Refer to Appendix D: Visual Impact Assessment for further details.

3.7 GREEN NETWORK

To protect Anmore's semi-rural character while fostering a more complete community, a Green Network has been identified to structure the Anmore South neighbourhood. Using the technical understanding assembled through topographic analysis and geotechnical, archaeological and environmental studies, the Green Network structures the neighbourhood towards ensuring the following:

- protecting the functional integrity of the natural systems;
- enhancing recreational opportunities for outdoor activities; and
- preserving the natural features that define the area's landscape character.

The Green Network is intended to balance the competing interests of neighbourhood development with environmental planning and is composed of the following two parts:

- Conservation + Recreation lands (21.3 ha | 52.7 ac)
- Neighbourhood Parks (4.3 ha | 10.7 ac)

Green Network:

Target approximately 42% (25.6 ha | 63.4 ac) of Anmore South.

Includes programmed neighbourhood parks, steep slopes (>30% grade), watercourses and typical Riparian Area Regulation (RAR) setbacks, wetlands, wildlife corridors, vegetation buffers from existing neighbourhoods and Sunnyside Road.

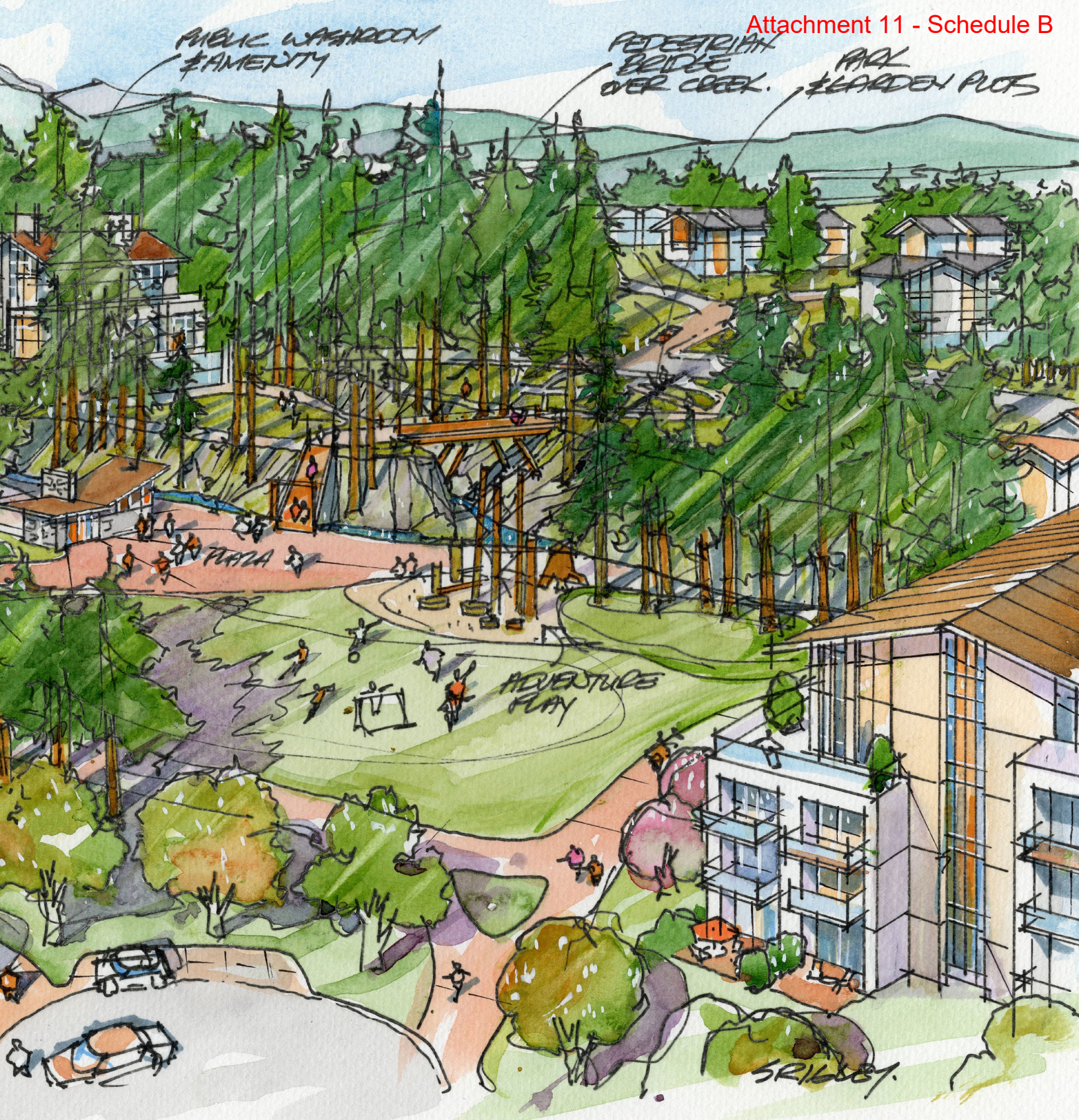
Neighbourhood Development:

Target approximately 58% (35.5 ha | 87.7 ac) of Anmore South.

Includes large congruent areas of gentle and hillside lands with major views and a southwest aspect, outside the defined riparian setbacks and identified retained forest buffers.

The above percentages are average targets with the final proportion between Green Network and Neighbourhood Development areas to be refined through future development applications. This may be through the findings of more detailed technical studies, such as development specific environmental, archaeological, and geotechnical reports.

4



Neighbourhood Land Uses

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4. NEIGHBOURHOOD LAND USES

The general land use designations for the Neighbourhood Plan - as illustrated in Schedule A: Land Use Plan, provide form and function to the vision for the community. This Section describes the land use designations, their planning rationale, and associated development policies to guide the development of Anmore South.

The Anmore South Land Use Plan consists of nine land use designations. Land use designations are broad categories of permitted land uses. A synopsis of the land use designations and areas is presented in Table 1 – Neighbourhood Plan Land Use Summary.

Table 1 – Neighbourhood Plan Land Use Summary

Land Use	Area (ha)	Area (ac)	% of Site	Description
Ground Oriented	1.84	4.55	3%	Single-Family and Duplexes intended to transition between existing neighbourhoods and Anmore South. Heights range up to 3 storeys.
Multi-Family Townhome	17.88	44.18	29%	Attached housing forms including duplexes, townhomes, stacked townhomes, and rowhomes. Heights range up to 4 storeys.
Multi-Family Apartment	3.90	9.64	7%	Mid-rise apartment buildings with integrated townhomes and stand-alone townhomes. Heights ranging from 2 to 6 storeys.
Mixed Use	1.50	3.70	2%	Mid-rise apartment buildings with integrated townhomes, office space, commercial retail units, and public realm.
Civic Community Centre	0.49	1.21	1%	Public Community Centre for use by all Anmore residents.
Civic Development Reserve	0.48	1.19	1%	Flexible reserve lands for future civic use.
Neighbourhood Park	4.34	10.72	7%	Programmed parks for active recreation and community gathering.
Conservation + Recreation	21.35	52.76	35%	Natural areas including retained forest buffers, watercourses, riparian areas, steep slopes, forest trails, and Village Greenways. May include uses for outdoor recreation and education.
Road ROW	9.36	23.13	15%	Public Street Right-of-Ways for pedestrians, vehicles, transit and active transportation.

Land use areas and percentages are approximate, intended for planning purposes only. All designated areas and boundaries are subject to refinement through more detailed site design and engineering at the time of zoning and subdivision.

4.1 GENERAL LAND USE POLICIES

Policy 4.1.1

LAND USE | Development should generally follow the Schedule A – Land Use Plan where the development pattern supports the Planning Principles utilized to create the Neighbourhood Plan. These are outlined in Section 2.2.

Policy 4.1.2

ILLUSTRATIVE CONCEPT | The illustrative land use concept for this site is shown on Figure 6 and is intended to guide a future rezoning application. For reference, where maps and illustrations identify building locations and shapes, they are intended to be representative only. Detailed building designs will be established through a future development permit application.

Policy 4.1.3

REFINEMENTS | Allow minor adjustments to land use designation boundaries as new information (e.g. geotechnical, environmental, archaeological) becomes available during detailed design at the time of zoning and subdivision, where proposed modifications support the Planning Principles and are approved by the Village of Anmore.

Policy 4.1.4

DEVELOPMENT PERMITS | Require Development Permits to be obtained for all developments in this neighbourhood in accordance with the applicable Development Permit Areas (Schedule: K) and associated guidelines (Section 8).

Policy 4.1.5

ARCHAEOLOGY | As recommended by the Archaeological Impact Assessment, all ground disturbing works within the Anmore South Neighbourhood Area are to be conducted under a Chance Find Procedure (refer to Appendix B).

Policy 4.1.6

MITIGATION + MONITORING | Environmental mitigation measures including environmental monitoring by a Qualified Environmental Professional must be conducted for all Anmore South development as specified in the Environmental Impact Assessment (March 2025) (see Appendix C).

Policy 4.1.7

ENVIRONMENTAL PERMITTING | Applicable Water Sustainability Act notifications or approvals are required for all changes in and about a watercourse or wetland.

Policy 4.1.8

GREEN NETWORK | Prioritize the function and integrity of Anmore South's Green Network (Neighbourhood Parks and Conservation + Recreation lands) through neighbourhood build-out.

Policy 4.1.9

GATHERING | Opportunities to create spaces where people can gather should be prioritized during the rezoning and development application process.

Policy 4.1.10

CHILD CARE | The neighbourhood shall accommodate child day care services where appropriate.

Policy 4.1.11

DARK SKY LIGHTING | Implement Dark Sky standards for all public streets and exterior lighting to limit ambient light and preserve the night sky.

Policy 4.1.12

INFRASTRUCTURE | Civil servicing infrastructure may be permitted in all Land Use designations.

Policy 4.1.13

STREET ROWS | Policies governing the Road ROW land use designation are described in Section 5 | Neighbourhood Infrastructure.

4.2 GENERAL RESIDENTIAL LAND USE POLICIES

The Neighbourhood Plan provides for a wide range of housing forms, from detached residential to townhouse and apartment, towards fostering a vibrant, socio-economic and age-mixed neighbourhood. The Neighbourhood Plan recognizes the need for a range of housing types, sizes and tenures to meet the needs of the community. A variety of housing choices provides a vibrant community experience, allowing for future growth with a range of lifestyles, life-stages and incomes.

Clustering development establishes a positive relationship between walkable residential enclaves and community destinations, while also providing access to parks and natural areas. Increased residential density serves to reduce infrastructure networks and maintenance costs, while allowing for greater retention of natural character, landscape and ecological systems.

Policy 4.2.1

HOUSING DIVERSITY | Encourage a variety of housing forms, lot sizes, and tenures to provide a diversity of residences that can support a range of lifestyles, life stages and incomes.

Policy 4.2.2

HOUSING YIELD | The maximum number of homes permitted in Anmore South is 1,750.

Policy 4.2.3

FOOTPRINT | Encourage compact housing forms, including small-lot single-family, duplexes, townhomes, and apartments, to foster walkable neighbourhoods and reduce construction materials and energy use.

Policy 4.2.4

HILLSIDE | Ensure that built forms respond to the hillside topography and manage the physical, visual and environmental impacts of hillside development, per the Hillside Development Permit Area (Refer to Section 8).

Policy 4.2.5

TRANSITIONS | Consider appropriate transition areas, landscape buffers, and/or other mitigating built form elements between adjacent land uses and housing types where there may be potential conflicts of scale and interface.

4.3 RESIDENTIAL – GROUND ORIENTED POLICIES

Maximizing the benefits of clustered development and the attributes of a walkable neighbourhood, the Ground Oriented designation only makes up a small portion of the total land use area. This includes a combination of detached single-family and two-family duplex homes. The location of this land use creates a transition of residential density from the existing larger lot homes adjacent to Anmore South.

The Ground Oriented designation provides development in keeping with existing suburban neighbourhood patterns of homes. With private individual yards and street frontage, single-family and duplex homes contribute to a strong public realm and support social interaction among neighbours.

Policy 4.3.1

DENSITY | Permitted gross densities per parcel within the Ground Oriented designation range from 4 units per acre (upa) to 10 upa.

Policy 4.3.2

ZONING | Zoning for the designation of Ground Oriented, shall be prepared as part of a future Comprehensive Development Zone for Anmore South.

Policy 4.3.3

FORM + TENURE | All housing within the Ground Oriented designation must be ground-oriented and shall be fee simple ownership, unless part of a bareland strata.

Policy 4.3.4

HILLSIDE | To ensure development fits the topography, development within the Ground Oriented designation is subject to a Hillside Development Permit Area (Refer to Section 8).

Policy 4.3.5

SERVICING | Lots in the Ground Oriented designation may be serviced by septic fields on a temporary basis but must ultimately be connected to the Anmore South sanitary system. Should this be desired, appropriate bonding as determined by the Village's Professional Engineer will be necessary to complete a connection to the Metro sanitary sewer for those units connected to the temporary system, at the Village's discretion.

4.4 RESIDENTIAL - MULTI-FAMILY | TOWNHOME POLICIES

Representing a significant portion of residential homes within Anmore South, the Multi-Family | Townhome designation affords opportunities for higher density urban living, while remaining ground-oriented and family-friendly.

The Multi-Family | Townhome designation is anticipated to include a variety of ground-oriented townhouse and duplex forms, serviced by private strata roads and provide common amenity spaces.

Policy 4.4.1

DENSITY | Permitted gross densities per parcel within the Multi-Family | Townhome designation range from 10 units per acre (upa) to 30 upa.

Policy 4.4.2

FORM | Housing within the Multi-Family | Townhome designation may include duplexes, townhomes, stacked townhomes, or row-homes.

Policy 4.4.3

SIZE | Provide for a range of attached housing choice including 2, 3, and 4 bedroom options.

Policy 4.4.4

TENURE | Permit the development of purpose-built rental, and/or strata tenure housing within the Multi-Family | Townhome designation.

Policy 4.4.5

TANDEM PARKING | Given Anmore's location, parking shall be provided for each unit in accordance with established standards. Limit the maximum percentage of tandem garage units within a Townhome development to 25% of the units in an individual project.

Policy 4.4.6

AMENITY | Provide private outdoor and indoor amenity space to enable social gathering and recreation within the strata property.

Policy 4.4.7

AMENITY | Allow for consolidated indoor amenity space that is communal among strata properties.

Policy 4.4.8

PLAY SPACE | Residential development is encouraged to provide outdoor children's' amenity space.

Policy 4.4.9

ACCESS | Where required, ensure public access can be maintained on strata property through the use of covenants or statutory right-of-ways.

Policy 4.4.10

WALKABLE | Sidewalks and parking areas are to be designed according to barrier free access standards.

Policy 4.4.11

LAYOUT | Consider the siting of buildings, entrances, and balconies as a means to frame views, define spaces and promote pedestrian activity.

Policy 4.4.12

LAYOUT | Encourage buildings to address public parks and streetscape as a means of creating a well-defined public realm.

Policy 4.4.13

FORM + CHARACTER | Promote high quality and complementary architectural form and character in keeping with the surrounding west coast community character.

Policy 4.4.14

ELECTRIC VEHICLES | Encourage electric vehicle charging stations and suitable parking for electric scooters and e-bikes as part of design for individual projects.

4.5 RESIDENTIAL - MULTI-FAMILY | APARTMENT POLICIES

The Multi-Family | Apartment designation makes up the highest proportion of units within the Neighbourhood Plan. This concentrated form of housing maximizes support of walkable neighbourhood shops and services, while providing access to public parks and retained natural areas. This land use is generally concentrated around key neighbourhood destinations, while also creating a more even distribution of density that celebrates the land's southwest views.

The Multi-Family | Apartment designation includes a range of low to mid-rise residential forms no more than six storeys. The designation may include townhomes within the ground floors to better activate street and park frontages, provide 'eyes on the public space', and contribute to greater housing diversity.

Policy 4.5.1

DENSITY | Permitted gross densities per parcel within the Multi-Family | Apartment designation range from 15 units per acre (upa) to 80 upa.

Policy 4.5.2

HEIGHT | Apartment buildings may be between 4-6 storeys. In hillside conditions the 6-storey maximum will be measured from the uphill side to allow for street-fronting townhomes on the downhill side.

Policy 4.5.3

FORM | All housing within the Multi-Family | Apartment designation may include apartment buildings with integrated townhomes in the building podium or stand-alone townhomes.

Policy 4.5.4

ZONING | Zoning for the designation of Multi-Family | Apartment shall be prepared as part of a future Comprehensive Development Zone for Anmore South.

Policy 4.5.5

SIZE | Provide for a range of apartment unit sizes, including units that have more than two bedrooms.

Policy 4.5.6

TENURE | Encourage the development of rental, and non-market rental within the Multi-Family | Apartment designation.

Policy 4.5.7

PARKING | Investigate opportunities to reduce parking requirements for apartments that promote and/or accommodate car-sharing and/or other forms of alternative transportation.

Policy 4.5.8

AMENITY | Provide private outdoor and indoor amenity space to enable social gathering and recreation within the apartment site. Prioritize the provision of roof top amenity space to enhance apartment livability and experience for building residents. Allow for indoor amenity space for multiple buildings to be consolidated in one or more locations.

Policy 4.5.9

PLAY SPACE | Residential development is encouraged to provide outdoor children's' amenity space.

Policy 4.5.10

PEDESTRIAN ORIENTED | Sidewalks and parking areas are to be designed according to barrier free access standards.

Policy 4.5.11

LAYOUT | Consider the siting of buildings, entrances, balconies and ground level uses as a means to frame views, define spaces and promote pedestrian activity. Where possible on ground floors, allow direct access to individual suites from street frontages.

Policy 4.5.12

LAYOUT | Encourage buildings to address the streetscape as a means of creating a well-defined public realm.

Policy 4.5.13

FORM + CHARACTER | Promote high quality and complementary architectural form and character in keeping with the surrounding west coast community character. Require implementation of Design Guidelines as means to reinforce local sense-of-place and character.

Policy 4.5.14

AMENITY | Encourage and facilitate the provision of shade and protection from the elements for pedestrians through landscape features (i.e., trees), awnings, overhangs, canopies and/or arcades over walkways.

Policy 4.5.15

SVFD | Ensure delivery of rental homes in the Apartment designation for use by Sasamat Volunteer Fire Department firefighters as part of a future rezoning process for these lands.

Policy 4.5.16

ELECTRIC VEHICLES | Encourage electric vehicle charging stations and suitable parking for electric scooters and e-bikes as part of design for individual projects.

4.6 MIXED USE POLICIES

The Mixed-Use designation is intended to create attractive, vibrant, pedestrian-friendly centres that serve as the social and commercial focus within the neighbourhood – all within walking distance of new and existing homes. Combining both residential and commercial uses within a landscaped public space, this designation supports aging-in-place through the provision of neighbourhood shops, services and recreational facilities on transit supportive routes. From restaurants to retail, office space to grocery store, this land use provides for the day-to-day needs of residents, while providing local employment and expanding the Village's tax base.

The Mixed-Use designation provides for neighbourhood shops and services, new local employment opportunities, and diversification of the municipal tax base. The designation accommodates a ground plain of commercial retail uses with underground parking, along with the opportunity for integrated townhouse and mid-rise apartment living above.

To support retail planning in Anmore South, a Village Retail Market Analysis was undertaken to identify the scale and mix of retail uses that could be supported based on trade area demographics, preliminary planning, and projected homes at build-out over ~25 years. The analysis projected that on completion Anmore South can support a retail floorspace of ~41,000 - 50,000 sq ft, including a grocery store, pharmacy, food + beverage, local services, clothing and hobbies or homewares.

This designation is distributed in two strategic locations:

CROSSING | The primary commercial node positioned at the confluence of Sunnyside Road and the proposed upper and lower collector roads. Situated between forested streams on a gentle terrace, the Crossing Mixed-Use node complements the public community centre and active park, creating a neighbourhood heart for Anmore South.

FIRST AVENUE | A secondary commercial node positioned along First Avenue near the western entrance to Anmore, supporting the needs of Belcarra residents and regional park visitors. As the terminus of the proposed Village Greenway, this node also provides direct access to active park space while serving the commercial needs of the neighbourhood's lower slopes.

Refer to Appendix E: Retail Market Analysis by City Squared Consulting for further details.

Policy 4.6.1

DENSITY | Permitted gross densities per parcel within the Mixed-Use designation range from 15 units per acre (upa) to 100 upa.

Policy 4.6.2

USES | Provide for a mix of potential commercial and employment uses including retail, public service, and professional office.

Policy 4.6.3

CROSSING | A neighbourhood Mixed-Use centre will be developed at the intersection of Sunnyside Road and upper and lower collector loops, which must include commercial retail, office space, and residential uses and is expected to provide ~41,000 - 50,000 sq ft of local serving retail.

Policy 4.6.4

FIRST AVENUE | Provide a Mixed-Use commercial node along First Avenue which may be developed as residential, commercial retail, office space or a mix of these uses.

Policy 4.6.5

SIZE | Provide for a range of commercial and residential unit sizes in the Mixed Use designation based on analysis at the time of rezoning.

Policy 4.6.6

COMMERCIAL PHASING | Phasing of commercial developments should be approved with consideration to the findings of the Anmore South Retail Market Analysis (February 2025) (see Appendix E).

Policy 4.6.7

TENURE | Encourage the development of rental and/or non-market housing within the Mixed Use designation.

Policy 4.6.8

PEDESTRIAN ORIENTED | Establish a ground level retail core within the Mixed-use designation to support a range of neighbourhood retail, commercial services and amenities within a pedestrian-oriented environment.

Policy 4.6.9

PEDESTRIAN ORIENTED | Ensure safe and secure pedestrian connections to Commercial Retail space through use of buffered sidewalks, multi-use pathways, marked street crossings and controlled pedestrian crossings.

Policy 4.6.10

PEDESTRIAN ORIENTED | Sidewalks and parking areas are to be designed according to barrier free access standards. Outdoor patios should be facilitated where appropriate.

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Policy 4.6.11

LAYOUT | Consider the siting of buildings, entrances, balconies and ground level uses as a means to frame views, define spaces and promote pedestrian activity.

Policy 4.6.12

LAYOUT | Encourage buildings to address the streetscape and plaza spaces as a means of creating a well-defined public realm.

Policy 4.6.13

AMENITY | Accentuate pedestrian connections to commercial and mixed-use developments through design features such as wayfinding signage, street design treatment and preserved view corridors.

Policy 4.6.14

FORM + CHARACTER | Promote high quality and complementary architectural form and character in keeping with the surrounding west coast community character. Consider implementation of Design Guidelines as means to reinforce local sense-of-place and character.

Policy 4.6.15

AMENITY | Encourage and facilitate the provision of shade and protection from the elements for pedestrians through landscape features (i.e., trees), awnings, overhangs, canopies and/or arcades over walkways.

Policy 4.6.16

AMENITY | Establish landscape buffers that create a transition between residential areas and adjacent commercial buildings and parking areas, through landscape design that incorporates layering of shrubs, trees, landscape structures and lower walls, as required.

Policy 4.6.17

ELECTRIC VEHICLES | Encourage electric vehicle charging stations and suitable parking for electric scooters and e-bikes as part of design for individual projects.

Policy 4.6.18

PARKING | Short term convenience parking should be provided.

Policy 4.6.19

SAFETY | Incorporate Crime Prevention Through Environmental Design principles into design of Mixed Use developments.

Policy 4.6.20

TENURE | Determine the tenure of commercial units as part of the future rezoning process.

4.7 CIVIC - COMMUNITY CENTRE POLICIES

The Institutional – Community Centre designation is intended to provide a new public facility for Anmore. The location of the Community Centre within the Neighbourhood Plan is convenient to all residents and establishes a prominent landmark near a mixed-use centre and Anmore Elementary School.

Anmore South's community centre is intended to provide for a range of community needs, from libraries to senior's and youth activity centres, day cares, community meeting and workspaces. With a focus on delivering community amenities with a walkable village scale, Anmore South's community centre can also support community programs including outdoor education and community healthcare delivery.

Within a 5-minute walk of Anmore Elementary, the Community Centre will feature safe accessible pathways, seating areas, and public washrooms, all set within an integrated parks and trail network.

Key features for the Community Centre will be determined by the Village but could include:

- Large multi-function flexi-hall for court sports, group programs, special events, and meetings;
- A foyer with crush space for gatherings, socializing, and art display;
- Multi-purpose spaces of various sizes;
- Fitness centre with a variety of fixed equipment;
- Additional dedicated use spaces such as studios, kitchen, games room, or social lounge;
- Associated outdoor amenities such as playground or splash park;
- Proximity to shops and services in a Mixed-Use Village;
- Connectivity to accessible pedestrian and cycling network; and,
- Within a 5-minute walk (400 m) of Anmore Elementary School and a 10-minute walk (800 m) of the Village Hub at the intersection of Sunnyside Road and East Road.

Policy 4.7.1

COMMUNITY CENTRE | Provide a public Community Centre within the designation for the use and enjoyment of Anmore Residents.

Policy 4.7.2

SIZE | The public Community Centre should provide an approximate floorspace of 16,000 sq ft.

Policy 4.7.3

ZONING | More specific permitted uses will be determined as part of the Comprehensive Development Zone for Anmore South.

Policy 4.7.4

CULTURE | Celebrate Anmore's local culture and history at the Community Centre through design and monumentation.

Policy 4.7.5

PUBLIC REALM | Encourage a vibrant public realm on the Community Centre lands, particularly where connected to public streets and Village Greenways.

Policy 4.7.6

SAFETY | Incorporate Crime Prevention Through Environmental Design principles into design of the Community Centre.

Policy 4.7.7

EMPLOYMENT | Consider opportunities for local businesses to utilize public Community Centre space through rental agreements.

Policy 4.7.8

CHILDCARE | Consider opportunities for child care at the Community Centre.

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4.8 CIVIC - DEVELOPMENT RESERVE

The Civic - Development Reserve designation provides an area which can be used by the Village to meet a variety of future land use requirements. This could include but is not limited to, additional active neighbourhood park space, non-market housing, a future firehall, infrastructure facilities, parking, or public works yard.

The Civic - Development Reserve designation has been strategically located within the Neighbourhood Plan, providing ease of access while avoiding interference with residential land use and maintaining a surrounding natural buffer.

This designation is distributed in two locations:

SUNNYSIDE CROSSING | This Civic Reserve connects directly onto Sunnyside Road at the eastern edge of Anmore South, providing a prominent place within the wider neighbourhood. This area could be ideal for supporting community functions with the adjacent Mixed Use and Community Centre land uses.

NORTHERN RESERVE | This Civic Reserve is accessed off the upper collector loop road and surrounded by a natural area buffer, making it suitable for future non-market residential uses or more discrete civic functions, like a public works yard.

Policy 4.8.1

PUBLIC DEDICATION | The lands should be dedicated to the Village of Anmore for the community's future use as part of a phased subdivision approach.

Policy 4.8.2

PERMITTED USE | Future uses of the Development Reserve lands are intended to be flexible with the future use to be determined by the Village.

Policy 4.8.3

SUNNYSIDE CROSSING | For the Development Reserve near Sunnyside Road, encourage future uses that complement the civic core established by the Community Centre and Mixed Use crossing, which may include active park, parking, or civic institutional uses such as a firehall.

Policy 4.8.4

NORTHERN RESERVE | For the Development Reserve in the northwest section of the neighbourhood, encourage future uses for community benefit, which may include non-market housing, active of natural park, or civic institutional uses such as a public works yard.

4.9 NEIGHBOURHOOD PARK POLICIES

Neighbourhood Parks seek to engage a diverse range of community needs, from quiet contemplative spaces for reflection to imaginative playgrounds and sports fields for active people. Serving as local gathering spaces, Neighbourhood Parks offer opportunities for both active and passive recreation, as well as community gardens, public art and outdoor classrooms, all within an easy walking and cycling distance of neighbourhood homes.

The Neighbourhood Plan provides 10.7 ac of Neighbourhood Park distributed across the neighbourhood. With access to parks and trails within a 5-minute walk (400 m) of every home, the Neighbourhood Park land use serves to create a healthy balance between development and natural open space and recreation.

Refer to Schedule B: Parks + Trails Plan for Neighbourhood Park distribution and connectivity within the Neighbourhood Plan.

Policy 4.9.1

PARKS | Anmore South must provide a network of public neighbourhood parks and gathering places, that interface with natural areas, streetscapes, and recreational and community facilities to enhance village vibrancy and livability, in accordance with Schedule A: Land Use Plan.

Policy 4.9.2

ACCESSIBILITY | Neighbourhood parks must be accessible for persons with disabilities or mobility issues, including those in wheelchairs.

Policy 4.9.3

PARKS PROGRAMMING | Public parks shall be designed and programmed to accommodate all age groups from children to seniors and will include both passive and active spaces as well as barrier free fully accessible circulation.

Policy 4.9.4

CONNECTIVITY | Ensure an interconnected network of pathways within public parks – including walking paths/trails, sidewalks and multi-use pathways – to the larger trail system, Village Greenways, and residential areas.

Policy 4.9.5

AMENITIES | Preserve future opportunities for the provision of public park amenities such as signage (for wayfinding and interpretation), benches, garbage receptacles and bicycle racks in public trailhead/ parking areas.

Policy 4.9.6

VIEWS | Establish destination points that make use of celebrated views such as areas for sitting, picnicking and informal lookouts.

Policy 4.9.7

PROGRAM | Preserve future opportunities to establish, maintain and improve appropriate active park programs (i.e. playgrounds, community gardens, off-leash dog parks, pump tracks, etc.) based on identified community need.

Policy 4.9.8

PUBLIC ART | Consider incorporating public art installations and outdoor performance facilities within parks and public spaces.

Policy 4.9.9

SPORTS FIELD | Provide a multi-use playing or sportsfield within the Neighbourhood Park on the south side of Sunnyside Road where it can be accommodated by gentle lands.

Policy 4.9.10

INVASIVE SPECIES | Implement policies to identify and manage invasive plants in parks.

Policy 4.9.11

STREETS | Provide public streets adjacent to parks to ensure visual and physical access and safety for the public.

Policy 4.9.12

LANDSCAPING | Encourage a landscaping palette that makes use of native plant species.

4.10 CONSERVATION + RECREATION POLICIES

The Conservation + Recreation land use is the neighbourhood's most significant component for preserving the existing character and identity values of the Village, as well as protecting significant ecological and recreation assets. The intent of the designation is to retain values related to the forested hillside including steep slopes, riparian areas, wildlife corridors, Visual Impact Buffers, trail corridors, and passive recreation opportunities.

Greenways + Trails

An integrated trail network offers active opportunities to experience nature, exercise and socialize with neighbours, and reach community destinations - all while leaving the car at home. Anmore South's proposed dedicated trail + pathway system is a defining feature of the neighbourhood, with ~5 km of dedicated multi-use trail. The trail network also provides an opportunity for community education through interpretive signage regarding cultural and ecosystem values to be celebrated (Refer to Schedule B: Parks + Trails Plan).

Village Greenway: the Village Greenways provide a separated 4 m wide pedestrian and cyclist multi-use pathway, offering safe off-street routes through forest that span the neighbourhood. Linking community destinations through a gentle accessible grade, the East-West Village Greenway connects the Mixed-Use Village and Community Centre at Sunnyside Road to Crystal Creek Drive in the northwest. A second North-South Village Greenway extends from Anmore Elementary School down through the village to the destination neighbourhood playfield and western gateway with First Avenue – including a safe crossing of Sunnyside Road (e.g., underpass).

Community Pathway: In support of the Village Greenways, additional multi-use Community Pathways (3 m wide) are provided to link individual Anmore South neighbourhoods to natural areas, civic destinations, and Anmore's existing trail system.

Refer to Figure 7C for conceptual multi-use path cross-sections. Specific configuration and programming of the Greenway + Trail Network will be determined through neighbourhood planning with community consultation.

Policy 4.10.1

AREA | A minimum of 35% of the neighbourhood should be preserved through dedication to the Village as Conservation + Recreation lands in accordance with Schedule A: Land Use Plan.

Policy 4.10.2

COMPONENTS | The Anmore South Conservation + Recreation Area is a central feature for the neighbourhood landscape and is to be excluded from development to retain values related to the forested hillside including steep slopes, riparian areas, wildlife corridors, Visual Impact Buffers, trail corridors and passive recreation opportunities.

Policy 4.10.3

WATERCOURSES | Ensure functional ecosystem protection of watercourses, including Doctors Creek and Schoolhouse Creek, their tributaries and ponds in accordance with industry standard best practices - including Develop with Care (2014).

Policy 4.10.4

ENVIRONMENTAL PERMITTING | Applicable Water Sustainability Act notifications or approvals are required for all changes in and about a watercourse or wetland.

Policy 4.10.5

RAINWATER | Preserve rainwater function within the Conservation + Recreation area designation in conjunction with recreational use, where feasible.

Policy 4.10.6

RESTORATION | Explore opportunities for habitat restoration and enhancement in the Conservation + Recreation area including streamside areas which collectively are significant components for rainwater management.

Policy 4.10.7

VISUAL BUFFERS | Avoid diminishment of significant areas of forest that act as Visual Impact Buffers between Anmore South and existing neighbourhoods unless it can be demonstrated that visual quality objectives are maintained through mitigation.

Policy 4.10.8

TRAILS | Generally, avoid developing trails in provincially regulated Riparian Areas Protection Regulation setbacks unless on Village-owned land and/or permitted by the Village.

Policy 4.10.9

CONNECTIVITY | Provide trail and greenway connections to existing neighbourhoods and Anmore Elementary School.

Policy 4.10.10

ACCESSIBILITY | Incorporate a range of accessible trail and pathway types to provide for a range of recreational uses and to maximize multi-modal connectivity, including 4 m wide off-street paved Multi-Use Paths where trails form critical connections in the pedestrian or cycling networks.

Policy 4.10.11

ECOSYSTEMS | Mitigate impacts to Valued Ecosystem Components due to trail and greenway construction through avoidance and/or appropriate mitigation strategies as recommended by a QEP.

5



Neighbourhood Infrastructure

5. NEIGHBOURHOOD INFRASTRUCTURE

The extension and provision of municipal infrastructure is fundamental to the realization of a mixed-use community in Anmore. The infrastructure defined by the Neighbourhood Plan accommodates the functional demand of the neighbourhood's land uses and population, incorporating measures to better sustain the community and lessen impacts on the natural environment.

5.1 TRANSPORTATION NETWORK

The Neighbourhood Plan's Transportation Network reflects connectivity and multi-modal transportation with context-sensitive streets. Each street and multi-use path is tailored to its purpose and connected as a network to allow choice and flexibility in moving through the community.

Aiming to reduce the proportion of automobile-based trips in the neighbourhood, the following active transportation design principles were considered:

- Implement Livable Streets Standards to lessen the physical, visual and environmental impacts of new streets. Refer to Figures 7A + 7B: Street Cross-Sections;
- Strive for integrated networks for pedestrian, cycling and vehicular traffic, including dedicated multi-use paths on all collector streets;
- Provide sidewalks on both sides of local streets; and,
- Provide trail connections between neighbourhood destinations, transit and cycling corridors.

Within the Transportation Network, measures to reduce greenhouse gas emissions and energy use include reduced pavement widths, tree-lined boulevards, rainwater infiltration galleries, and dedicated multi-use paths that contribute to the pedestrian and cycling networks.

Livable Streets

Sharing is central to the design of 'livable streets' that serve everyone's needs, from the weekday commuter to the weekend stroller. Livable streets are those that are well suited to their purpose of making a place for everyone that is functional, safe, and enjoyable. In the spirit of sharing, and to reduce reliance on private automobiles, Anmore South's Livable Streets enhance the pedestrian experience, improve bicycle safety, and make for more efficient traffic circulation.

Street Network and Hierarchy

The topography of Anmore South serves to limit the options available for establishing a traditional network of public neighbourhood streets. The proposed Major Street Network works to lessen the physical, visual, and environmental impacts of streets through site sensitive design, including landscape buffering and traffic calming.

Collector Streets provide the primary vehicle circulation for the neighbourhood, including an Upper and Lower Loop from Sunnyside Road. Designed to be supportive of pedestrians, cyclists, and future transit service, the Collector Streets link the neighbourhood's residential areas to neighbourhood parks, the Sunnyside Crossing Mixed Use Centre, and the Major Road Network.

Local Streets provide a complete pattern of streets and convey the vision for the neighbourhood. Local Streets also serve to enhance the Village's street network by connecting Crystal Creek Drive to the Anmore South collector loop and connecting Fern Drive to Sunnyside Road through adjacent neighbourhood development.

Refer to Schedule D: Street Hierarchy Plan.

Pedestrian + Cycling Network

Anmore South's Pedestrian Network links all homes to active parks, natural recreation areas, and civic destinations including Anmore Elementary School and the new Anmore South Community Centre. The Pedestrian Network includes 4 m wide Village Greenways linking Anmore South with existing neighbourhoods and community destinations, enabling pedestrians to experience the retained forest character of Anmore South's Conservation + Recreation lands. Village Greenways have been designed at a gentle grade suitable for all users and include a safe pedestrian underpass of Sunnyside Road. In addition to the Village Greenways, 3 m wide Community Pathways are provided throughout Conservation + Recreation lands, providing additional pedestrian permeability throughout the neighbourhood. Anmore South's street network also provides pedestrian sidewalks on both sides of all Collector and Local Streets.

The Cycling Network supports multi-modal transportation by providing a safe and efficient alternative to the traditional vehicle commute. The 4 m wide Village Greenways include a separated 2.2 m wide bicycle path for safety of cyclists and pedestrians. Additionally, all Collector Streets include a grade-separated off street cycling path on one side of the street.

Refer to Schedule E: Pedestrian Network Plan

Transit Network

Expanding local transit service into the Neighbourhood Plan area is an important part of creating a multimodal community, with future residential density playing an important role in the type of transit to be provided. Anmore South is within a 10-minute drive of rapid transit in Port Moody. However, the current level of transit service in Anmore provides limited coverage because of its existing low population density.

Anmore South's residential densities are supportive of increased transit service, with the objective of providing improved transit connection to Suter Brook Village, the SkyTrain, and West Coast Express.

Residential densities at Anmore South are supportive of increased frequency of TransLink's current Route 182, providing a community shuttle bus from SkyTrain and commuter rail at Moody Centre Station via East Road, Sunnyside Road, and Bedwell Bay Road.

TransLink's Route 181 provides a second and more direct service along Ioco Road but currently terminates before Anmore South at Ioco Road & First Avenue. The Neighbourhood Plan Street Network provides for safe bus share facilities as well as a looping Collector Street suitable for extending Route 181 to access the Sunnyside Road Commercial Crossing and new Anmore South Community Centre.

All improvements to transit service will require collaboration between the Village of Anmore and TransLink. Refer to Schedule F: Transportation Plan.

Transportation Impact Assessment

Transportation Impact Assessment Report

March 2025

Bunt & Associates

A detailed Transportation Impact Assessment was prepared by Bunt & Associates to assess traffic management, alternative transportation, and the capacity of the street network within Anmore South and its effect on the wider area.

The Transportation Impact Assessment identified the following:

- Anmore South will advance the Village's rural transportation network, by providing new multi-modal street connections to the existing network at Sunnyside Road, Crystal Creek Drive, and Fern Drive.
- Road and intersection capacity was calculated with increased traffic volumes that assumed the full buildout of Anmore South, along with an annual 1% increase in background traffic growth (from other development in the area).
- Roads and intersections within Anmore are expected to be able to accommodate this increase, subject to achievable upgrades at three identified intersections, such as roundabouts or signalization.
- Regional roads leading into Anmore (Ioco Road and East Road) and intersections along these corridors are expected to be overcapacity with this increase. Future improvements would involve discussions with TransLink, Metro Vancouver and neighbouring municipalities. Phased analysis estimates that without these future improvements, approximately 880 new homes can be accommodated with the current regional road network.

Recommendations to Reduce Traffic Impacts

The Transportation Impact Assessment recommends the following Transportation Demand Management (TDM) measures to be implemented to offer people viable travel alternatives to their private vehicle. These measures include:

- Sidewalks on all streets, plus multi-use paths and trails for car-free walking and cycling routes to neighbourhood parks, community centre, and commercial stores.
- Reduced parking ratios in all phases of development, naturally catering to households with fewer vehicles.
- Car-share program (20 vehicles planned to be phased, including specialized vehicles).
- Neighbourhood shops and civic services to introduce the option to stay local for many trips.

Funding and discussions with TransLink to enhance public transit and bus shelter facilities:

- Phase 1: Extend bus route 181 from Ioco Road to loop into Anmore South with current frequencies to provide a more direct connection to Port Moody.
- Full Buildout: Frequent bus service (15-minute intervals all week), which could involve an express bus to Suter Brook/Newport and Port Moody rapid transit locations.

Ongoing monitoring is also recommended through updated Transportation Impact Assessments after each 300 new housing units, which helps account for the uncertainty in background traffic growth and reassess any impacts. This allows for the incremental development of the Anmore South lands within the means of the evolving transportation networks. These studies will determine if/when the following recommended upgrades should be built to reduce the effects of increased traffic:

- Small-scale upgrades to slightly increase capacity and reduce delay on the two existing access corridors to Anmore that presented operation constraints – East Road and Ioco Road. These could include adding left turn lanes to high-volume locations, designated bus pullout stops at high ridership locations, removing on-street parking where possible, and introducing or expanding traffic signals at high-volume locations.
- Intersection upgrades within Anmore at East Road/Sunnyside Road, First Avenue/Ioco Road, and First Avenue/Bedwell Bay Road/Sunnyside Road.
- Further collaboration with TransLink, Metro Vancouver, and local municipalities to discuss transit improvements and develop a coordinated approach to transportation planning on regional roads, including road capacity in Anmore, Belcarra and Port Moody.

5.2 STREET NETWORK AND TRANSPORTATION POLICIES

Policy 5.2.1

TRAFFIC MANAGEMENT | incorporate the recommendations for traffic management and parking requirements presented in the Anmore South Transportation Impact Assessment.

Policy 5.2.2

TRAFFIC MONITORING | As recommended by the Transportation Impact Assessment, require an updated traffic assessment with new monitoring data after each 300 new housing units are completed.

Policy 5.2.3

LIVABLE STREETS | Implement the Livable Street cross sections as shown in Figures 7A + 7B to limit physical, visual, and environmental impacts while providing multiple street connections with relatively direct routes.

Policy 5.2.4

IMPACTS | Minimize the impact of streets within residential neighbourhoods through site sensitive, pedestrian oriented design, including landscape buffering and traffic calming.

Policy 5.2.5

LOCAL STREETS | Further develop and refine local street alignments through the subdivision process.

Policy 5.2.6

EMERGENCY CONNECTOR | Provide emergency connectors (vehicular) with pedestrian use for cul-de-sac road lengths greater than 200 m in length.

Policy 5.2.7

PUBLIC REALM | Create a positive pedestrian realm through the provision of pathways, street trees, landscaping, pedestrian scale lighting, street furniture and permeable surface treatment where possible.

Policy 5.2.8

PUBLIC REALM | Preserve future potential for public pedestrian amenities such as universally accessible pathways, benches, and street trees along new major streets.

Policy 5.2.9

OFF-STREET NETWORK | Construct an off-street network to accommodate walking and cycling, including trails, multi-use pathways and Village greenways through the development process.

Policy 5.2.10

OFF-STREET NETWORK | Where a multi-use pathway is constructed off-street, ensure appropriate lighting is provided for safety, applying Dark Sky lighting principles.

Policy 5.2.11

END-OF-TRIP FACILITIES | Incorporate end-of-trip facilities for cyclists, including secure bicycle storage and/or bike racks in highly visible locations at destination civic centres and mixed-use developments.

Policy 5.2.12

TRANSIT | Design all collector roads to accommodate future transit service.

Policy 5.2.13

TRANSIT | Accommodate the provision of transit service through planning for future facilities and/or physical design requirements (transit exchange/bus stops), particularly at the Community Centre and Commercial Crossing along Sunnyside Road.

Policy 5.2.14

TRANSIT | Collaborate with TransLink as development progresses and explore opportunities for the Independent Transit Service program as well as increased transit service to the Village.

Policy 5.2.15

LIGHTING | Explore alternative street lighting standards to promote energy efficiency and limit ambient light pollution in support of Dark Sky Lighting principles.

Policy 5.2.16

PARKING | Provide parking for destination parks and trailheads through detailed park design.

Policy 5.2.17

CRYSTAL CREEK | Provide a new local street connection to Crystal Creek Drive to connect the Crystal Creek neighbourhood with the rest of Anmore.

Policy 5.2.18

FERN DRIVE | Provide a new local street connection from Fern Drive to Sunnyside Road through adjacent development.

Policy 5.2.19

TDM | Transportation Demand Management measures should be explored in conjunction with a future rezoning application as outlined in the Anmore South Transportation Impact Assessment (March 2025) (see Appendix F).

Policy 5.2.20

TRUCK ROUTES | Truck movement will be consistent with the Regional Goods Movement Strategy and Regional Truck Route Network.

5.3 MUNICIPAL SERVICING INFRASTRUCTURE

Infrastructure Servicing Report

January 2025

Aplin Martin Consultants Ltd.

The conceptual Water, Sanitary, and Rainwater Systems proposed for Anmore South respond to its unique location and topography. Accommodating the planned servicing demands, the infrastructure concepts are cost effective, respectful of the environment, and designed to conserve water and energy resources. Servicing plans described in this Neighbourhood Plan are conceptual for the purpose of documenting neighbourhood feasibility and are subject to change through detailed design.

Anmore South Conceptual Water System

The conceptual Water System is designed to connect Anmore South to the regional trunk water infrastructure. Reflecting the complex topography of Anmore South, the water system is divided into pressure zones served by a reservoir located at a higher elevation. The water system will consist of feeder and distribution mains, booster pump stations, pressure reducing valves, and proposed blowoffs at each of the end pipes. The final water system design will be determined through detailed design as part of the rezoning and approvals process.

Refer to Schedule G: Water Servicing Concept.

Anmore South Conceptual Sanitary System

The conceptual Sanitary System is designed to connect Anmore South to the regional trunk wastewater infrastructure. The layout will rely on gravity flow through a combination of street alignments and dedicated utility corridors, directing flows toward the regional connection point. In areas where gravity service is not feasible, low-pressure systems will be utilized. The final sanitary system design will be determined through detailed design as part of the rezoning and approvals process.

Refer to Schedule H: Sanitary Servicing Concept.

Anmore South Conceptual Rainwater Drainage System

Anmore South anticipates a layered rainwater management concept that adheres to best practices to address both water quality and quantity issues. Rainwater will be retained and detained with on-street retention and infiltration, and neighbourhood detention prior to being released into natural creeks.

The conceptual Rainwater Drainage System collects and slows precipitation, allowing for infiltration and recharge prior to releasing water back to on-site streams and wetlands. Post-development flow rates are intended to be limited to peak pre-development flow rates using detention facilities in order to protect creeks and aquatic habitat from high velocity flows.

The final rainwater system design will be determined through detailed design as part of the rezoning and approvals process.

Refer to Schedule I: Drainage Concept.

5.4 SERVICING INFRASTRUCTURE POLICIES

Policy 5.4.1

INFRASTRUCTURE SERVICING | The neighbourhood will be serviced generally in accordance with the Anmore South Neighbourhood Plan Infrastructure Servicing report dated March 2025 and included as Appendix G.

Policy 5.4.2

REGIONAL SERVICE | Anmore South will be connected to regional drinking water and sanitary services following approval from Metro Vancouver. Regional sanitary service will be limited to the Anmore South neighbourhood to preserve the semi-rural density and character of existing Anmore.

Policy 5.4.3

CONSTRUCTION | The developer will finance new trunk servicing infrastructure connecting the Metro Vancouver water and sanitary systems.

Policy 5.4.4

OWNERSHIP | The Village will own and operate distribution infrastructure facilities in the neighbourhood and access to them will be enabled. Metro Vancouver will own and operation the regional infrastructure.

Policy 5.4.5

RESERVOIR | The final location of the new water reservoir will be determined at the rezoning stage in conjunction with Anmore Staff to optimize location.

Policy 5.4.6

RAINWATER | Implement rainwater management systems through detailed design that support pre-development surface and groundwater flow regimes in natural areas to the extent feasible.

Policy 5.4.7

FIRE FIGHTING | Explore opportunities to design water servicing infrastructure to support wildland fire prevention and fire fighting.

Policy 5.4.8

OPTIMIZATION | Allow for flexibility in infrastructure servicing plans through detailed design to maximize community benefit.

6



Neighbourhood Economics

6. NEIGHBOURHOOD ECONOMICS

6.1 FISCAL + ECONOMIC ANALYSIS

Fiscal Impact Analysis

April 2025

Deloitte

The Fiscal Impact Analysis was prepared by Deloitte to assess the net fiscal impacts of Anmore South on the Village of Anmore. The analysis was designed to test the financial feasibility of the project using the Village's current levels of services for staffing, amenities and infrastructure

The analysis compared the following two alternative scenarios:

- Anmore's current financial situation, based on the 2024 Village budget as outlined in the 2024 Five-Year Financial Plan; and,
- Anmore South neighbourhood at full buildout.

This approach allowed for a simple and straightforward comparison of Village of Anmore finances with and without Anmore South by eliminating the uncertainty of the timing of development, cost inflation over time, changing Village priorities and initiatives, and other developments that are unrelated to Anmore South.

The analysis took into account revenues from municipal taxes, grants, penalties and interest, licence fees, and miscellaneous income, compared with cost impacts of government, public works, fiscal services, policing, fire service, and infrastructure lifecycle costs.

Anmore South will generate a net fiscal benefit for the municipality through new residential and commercial property taxes. While the estimated specific net benefit differed between Deloitte and the Village's consultant, the analysis identified a net fiscal benefit. This improvement provides the Village with expanded financial options, ranging from new or improved services to accelerated infrastructure renewal to tax reduction.

In addition to net taxation benefit, Anmore South will generate significant revenue for the Village through development cost charges, while also creating opportunities for local businesses to serve the future needs of the growing community.

Refer to Appendix H: Fiscal Impact Analysis for further details.

Economic Impact Analysis

March 2025

Deloitte

The Economic Impact Analysis was prepared by Deloitte to assess the projected economic impact of the proposed Anmore South neighbourhood on the Village of Anmore. The report summarized the economic impacts of Anmore South as they relate to employment, demographics and housing.

The analysis considers impacts on employment resulting from the proposed commercial uses, civic facilities, and expanded range of housing. Based on the analysis, Anmore South is estimated to have a positive local employment impact of 340 jobs. This includes jobs based in Anmore South at the grocery store, specialty food stores, community centre, childcare facilities, home-based employment, and local government and schools.

In addition to the ongoing impacts from a fully built-out Anmore South, the analysis identifies significant employment generated by project construction. Using preliminary construction cost estimates, and input-output multipliers from Statistics Canada, and assuming that at least 90% of the expenditure is carried out by Metro Vancouver firms, the direct construction employment associated with the project is an estimated 2,800 person-years.

Refer to Appendix I: Economic Impact Analysis for further details.

6.2 FISCAL + ECONOMIC POLICIES

Policy 6.2.1

LOCAL SERVICE AREA | A Local Service Area shall be established to ensure that fees associated with the regional sanitary sewer are borne by residents and commercial users benefiting from the sewer connection.

Policy 6.2.2

EMPLOYMENT | Maximize local job opportunities to enhance Anmore's economy in accordance with the Economic Impact Analysis (March 2025) (see Appendix I).

Policy 6.2.3

DEVELOPMENT COST CHARGES | The Village shall use Development Cost Charges to ensure that development - not existing Anmore taxpayers - pays the costs of future infrastructure upgrades as identified by the Village.

Policy 6.2.4

COMMUNITY AMENITY CONTRIBUTIONS | Community Amenity Contributions as determined by the Village must be provided for Anmore South in accordance with a future rezoning agreement to provide community benefits.

7



Neighbourhood Implementation

7. NEIGHBOURHOOD IMPLEMENTATION

7.1 NEIGHBOURHOOD PHASING STRATEGY

Anmore South is projected to build-out through a phased development over 20-25 years to accommodate community needs, while respecting the semi-rural character of the existing village. Allowing for an orderly provision of community development and services, the Neighbourhood Plan provides a high-level framework for phasing. Future detailed planning will provide analysis and recommendations on refined neighbourhood phasing.

Anmore South will be phased to ensure that the infrastructure and services required to support the growth of the new community are in place in phases. The neighbourhood is expected to develop according to the following sequence of phases:

- Phase 1 | Fernwood
- Phase 2 | Ridge
- Phase 3 | Glade
- Phase 4 | Crossing
- Phase 5 | Terrace
- Phase 6 | Creekside

The phasing strategy is to be further developed during the rezoning and development agreement process, also involving external agencies, including School District 43 and utility companies as they plan future infrastructure and facility decisions.

Refer to Schedule J: Conceptual Phasing.

7.2 IMPLEMENTATION POLICIES

Policy 7.2.1

REGIONAL DESIGNATION | The Anmore South neighbourhood will be fully within the regional Urban Containment Boundary following consideration by Metro Vancouver.

Policy 7.2.2

PHASING | Development within the Anmore South neighbourhood will be gradual and on a phased basis.

Policy 7.2.3

STREET STANDARDS | Livable Street Standards for Anmore South must be prepared by a Professional Engineer and accepted by Village staff to provide engineering design criteria for the Collector and Local street cross-sections presented in Figure 7A + 7B.

Policy 7.2.4

SERVICING | Detailed engineering plans and infrastructure cost estimates for linking water and sanitary services to Anmore South must be prepared by a Professional Engineer.

Policy 7.2.5

DEVELOPMENT PERMITS | Applicable Development Permits must be obtained for all Anmore South development in accordance with Development Permit Areas and guidelines described in Section 8 of the Neighbourhood Plan.

Policy 7.2.6

ZONING | Implementing zones for Land Use Designations shall be prepared as part of the future Anmore South Comprehensive Development Zone.

Policy 7.2.7

SD43 | The Village will continue to work with School District 43 on servicing the expected population growth in the Anmore South Neighbourhood as well as potential facility improvements at Anmore Elementary.

Policy 7.2.8

GHG Reduction | Encourage use of energy efficient technologies and building materials in Anmore South developments.

Policy 7.2.9

DESIGN GUIDELINES | Design Guidelines should be prepared by the master developer to ensure quality and consistency of neighbourhood development over build-out.

8



Development Permit Areas

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8. DEVELOPMENT PERMIT AREAS

8.1 DEVELOPMENT PERMIT AREAS + GUIDELINES

Under the British Columbia Local Government Act, an official community plan may designate Development Permit Areas (DPA) where they relate to the purposes set out in Section 488 (1). The following purposes are applicable to the Neighbourhood Plan, as they relate to the type of proposed development, respond to existing site conditions, and will help preserve the existing character of Anmore:

- **Section 488 (1) (a):** protection of the natural environment, its ecosystems and biological diversity;
- **Section 488 (1) (b):** protection of development from hazardous conditions; and,
- **Section 488 (1) (f):** establishment of objectives for the form and character of commercial and multi-family residential development.

To address the purposes in accordance with the Local Government Act, the Neighbourhood Plan therefore includes the following DPAs:

- **Development Permit Area 1:** Multi Unit Residential (DPA 1);
- **Development Permit Area 2:** Commercial Mixed Use (DPA 2);
- **Development Permit Area 3:** Natural Environment (DPA 3); and
- **Development Permit Area 4:** Hillside Development (DPA 4).

These DPAs will guide development in areas with special conditions. Development within DPAs will require submission of a Development Permit Application to the Village of Anmore with application requirements specific to each DPA. As not all applications warrant the same level of detail, a pre-application meeting with the Village will be required to determine the application requirements.

The following sections describe the purpose and justification for the inclusion of these DPAs within Anmore South. For DPA 1 and 2, high level objectives are set guiding the overall tone and key requirements for development within these DPAs. However, currently specific design guidelines are understood to be premature for setting the form and character of built form. A comprehensive list of design guidelines will be detailed at the time of rezoning and subdivision to further guide development under these DPAs.

DPA 3 and 4 are site conditional, relating to existing constraints within Anmore South and are for the protection of the natural environment as well as future development. As such, design guidelines can be set and are outlined within the Neighbourhood Plan.

It should be noted that a specific development site may require multiple development permits to facilitate development.

See Schedule K: Development Permit Area Plan.

8.2 DPA 1: MULTI UNIT RESIDENTIAL

Purpose of Designation Category

Pursuant to subsection 488 (1) (f) of the Local Government Act, the purpose of this designation is to establish objectives for the form and character of multi-family residential development.

Justification

The Multi Unit Residential DPA is applicable to all duplex, townhome and apartment building forms in Anmore South within the Residential – Multi-Family | Attached, Multi-Family | Apartment and Mixed-use land use designations. This DPA is designated to establish high level guidelines for the form and character of multi unit residential development. This is with the intention to achieve a higher standard of building design, housing alternatives, site compatibility and site aesthetics that are consistent with a vibrant residential neighbourhood meeting the following objectives:

- to ensure that developments are compatible in scale, form and character with the adjacent uses and consistent with the overall neighbourhood plan;
- to encourage developments that serve to preserve and enhance any special natural, or aesthetic features which help define the identity of the area;
- to provide ease of access for residents, regardless of physical capabilities;
- to ensure that, where necessary, the design creates suitable transition between differing land uses or residential densities;
- to ensure that multi-family development is designed to provide the features and amenities suitable for the needs of future residents.

Multi-Unit Residential Development Permit Guidelines (DPA 1):

General Design

1. CHARACTER | The character of multi-family development at Anmore South will generally:
 - a. celebrate and respect the west coast character;
 - b. be designed to integrate into the existing topography and natural landscape;
 - c. incorporate a variety of local, natural building materials into the design of the buildings wherever feasible;
 - d. be designed to respond to the local climate and situated to maximize views and natural sunlight wherever feasible; and,
 - e. encourage buildings to address the street, or greenway/other public space or both, to create a highly permeable public realm edge and allow direct access to suites from ground elevations where possible.

2. **OUTDOOR SPACE** | Outdoor patios, landscaped commons and/or amenity areas are encouraged.
3. **STREET FURNITURE** | Street furniture, such as benches, light fixtures, bicycle racks, signage, and recycling/refuse containers, shall be incorporated in the landscape design. These shall be consistent, similar, or identical in character to the architectural character of the development. A standard shall be established as part of a future rezoning application.
4. **PEDESTRIAN** | Public open space and pedestrian walkway linkages to adjacent neighbourhoods (to complement recreational opportunities and reduce automobile dependence) shall be encouraged.

Building Siting, Height, Size & Massing

5. **SITING** | Building placement and design shall consider setting buildings into the hillside and stepping upper storeys back to respect views from the adjacent buildings where feasible.
6. **MASSING** | Buildings shall be designed to avoid presenting an overly dominant appearance using varying architectural massing, roof line and balcony/terrace design, window treatments, and landscaping to reduce monolithic forms and improve their aesthetic appearance.
7. **ARCHITECTURE** | Human-scaled architectural elements are required to minimize the visual impact of larger multi-unit forms. This could involve framing taller multi-family buildings with 2 to 3 storey structures at street level.
8. **OVERHANGS** | Deep overhangs and/or covered balconies and patios adjacent to the primary living areas shall be provided where feasible.
9. **SHELTER** | Continuous weather protection over main entrances and over building facades shall be encouraged.
10. **SHADING** | Building siting and internal spaces should be designed to promote natural ventilation, reducing reliance on mechanical means. Shading devices should be used to control solar heat gain during summer months while permitting sunlight into living space during the cold months.
11. **ROOFS** | Stepped roof lines that scale buildings from major to minor elements are strongly encouraged.

Materials & Exterior Finishes

12. MATERIAL | The use of non-combustible building materials is encouraged.
13. MATERIAL | Where feasible, a variety of locally responsive and/or natural building materials should be incorporated into the design without compromising the building or structure's fire resistance.
14. MATERIAL | Select exterior building materials for their functional and aesthetic quality. These materials should exhibit high qualities of durability, longevity and ease of maintenance.
15. MATERIAL | Continue higher quality materials used on the principal façade around any building corner or edge which is visible to the public.
16. FINISHING | Authentic detailing and application of exterior finishes is strongly encouraged. Unfinished building walls, including exposed basements, are discouraged.
17. COLOUR | Colours shall be selected to complement the natural environment, inspired by site vegetation and vistas: earth-based warm greys, browns and umbers, and moss greens as well as cooler colours inspired from the sea and sky, such as pale blues and greys. Lighter tones can be used to provide accent trim and in base areas, projecting elements and entries. Bolder colours will be acceptable, assuming the overall palette is complementary.

Parking and Loading

18. LANDSCAPE | Parking areas should be integrated with the topography where feasible consisting of a series of smaller parking areas, screened through landscape design to establish a pedestrian-friendly environment while reducing the visual impact of surface parking areas. Underground parking is preferred, where feasible
19. ORIENTATION | Where feasible, parking and loading areas shall be to the rear of the front-face of buildings.
20. LOADING | Shared loading areas will be considered when compatible uses are in the same development.

Landscaping and Screening

21. LANDSCAPE | Landscape Plans shall be prepared by a BCSLA landscape architect and shall satisfy the following objectives:
 - a. to use a variety of native or similarly hardy, drought tolerant deciduous and evergreen plant species, perennials and grasses that are best suited to the site specific growing conditions;
 - b. to minimize water consumption through means such as micro-irrigation and xeriscaping;
 - c. to provide visual separation from and compatibility with surrounding single family dwelling uses;
 - d. to improve the aesthetic appeal of the development;
 - e. to assist in the safe movement of pedestrians throughout the site while limiting access to sensitive areas;
 - f. to reduce the amount of impervious surfaces on the site;
 - g. to complement the development and surrounding uses;
 - h. to preserve natural character and delineate between amenity space and natural areas; and,
 - i. to establish or enhance habitat values on the development site where appropriate.
22. VEGETATION | Retention of natural vegetation is encouraged wherever possible both during construction and throughout the life of the development.
23. VEGETATION | Plant species used in replanting, restoration and enhancement shall be selected to suit the soil, light and groundwater conditions of the site with temporary irrigation required, should be native to the area, and be selected for erosion control and/or habitat values as needed.
24. RAINWATER | Low impact rainwater control measures shall be integrated into paving treatments and landscape design to encourage detention and improve water quality.
25. BUFFERS | Where appropriate, establish landscape buffers that create a natural transition between the public, semi-public and private uses through the use of hard and soft landscaping, such as areas containing layers of shrubs, trees and low walls where required.

- 26. **GARBAGE** | Garbage and recycling containers (bear/animal resistant), utilities, service kiosks, meters, elevator housing, exhaust elements, satellite dishes, etc. shall be screened or enclosed with a combination of landscaping, trees, fencing and gates to a minimum height of 2.0 metres. Any outside storage areas shall be located to the rear of buildings unless adequately screened.
- 27. **FENCING** | Chain link fencing shall be used only when screened by landscaping. Decorative fences are encouraged.
- 28. **FOUNDATIONS** | Exposed foundation walls should match the exterior finish of the building – including parkade structures and/or exposed basements.
- 29. **INTERSECTIONS** | Landscaping at intersections shall protect visual triangles.

Pedestrian + Cycling Considerations

- 30. **PATHS** | Pedestrian sidewalks and pathways should provide direct/ convenient connections between building entrances, parking areas and sidewalks/pathways of adjacent streets.
- 31. **ACCESS** | Sidewalks and parking areas designed according to barrier free access standards are required.
- 32. **SIGHTLINES** | Walls, fences, landscaping, grade changes or other site features should not obscure vehicle driver vision of pedestrian or bicycle routes or provide for concealment.
- 33. **PEDESTRIAN** | Where appropriate, pedestrian facilities shall be provided to separate pedestrian and traffic circulation on a site and minimize vehicle/pedestrian conflicts.
- 34. **AMENITIES** | Public and private street designs shall provide pedestrian amenities such as benches, human-scaled lighting, street trees, and recycling/refuse receptacles where appropriate.
- 35. **SAFETY** | Safe pedestrian routes shall be provided to link commercial and multi-unit residential developments to and through existing neighbourhoods, parks and neighbourhood destinations.
- 36. **PATHS** | All internal pedestrian walkways shall be distinguished from driving surfaces through the use of attractive clearly delineated pathway or durable, low maintenance surface materials such as pavers, bricks, or concrete with consideration of pedestrian safety, abilities and comfort.

Site Illumination + Signage

37. SIGNAGE | Signage shall complement the design of buildings and structures and be grouped, where possible, in multiple development sites. The use of natural materials is encouraged. A consistent signage approach is encouraged.
38. SIGNAGE | The size, location and design of freestanding signage shall be architecturally integrated with the overall design of the buildings and landscaping. The design of fascia signs containing individual signage shall be integrated into the design of the building.
39. SIGNAGE | No roof top signs shall be permitted. Multi-tenant buildings shall provide combined tenant signage.
40. SIGNAGE | Signage should be visually unobtrusive; signage should be designed to be aesthetically pleasing and require a minimal amount of lighting or boldness to be effective.
41. LIGHTING | The use of indirect and/or accent lighting on signage is encouraged.
42. DARK SKY LIGHTING | Lighting should be designed, both outside and inside developments, in accordance with Dark Sky guidelines to improve safety, minimize glare and preserve the ambiance of the night sky. The use of video signage is not permitted.
43. LIGHTING | All new, replacement and upgraded exterior lighting in existing and proposed developments shall use Full-Cut Off/Flat Lens (FCO/FL) luminaires as required for roads, parking, loading and pedestrian areas. Exterior building lighting will also be required to use FCO lighting fixtures.

8.3 DPA 2: COMMERCIAL MIXED USE

Purpose of Designation Category

Pursuant to subsection 488 (1) (f) of the Local Government Act, the purpose of this designation is to establish objectives for the form and character of commercial development.

Justification

The Commercial Mixed Use DPA is applicable to all properties in Anmore South that contain a mix of residential and commercial related uses within the Mixed-Use land use designation. The intention is to achieve a higher standard of building design, housing alternatives, site compatibility and site aesthetics that are consistent with vibrant community nodes and commercial areas with the following objectives:

- to ensure that commercial development contributes to the economic vitality of the area and the creation of a more complete community, as well as, remaining sensitive to the residential component in mixed-use buildings;
- to ensure that development is designed to reduce the reliance on vehicles and enhance the pedestrian environment;
- to ensure that multi-family development respects the character of surrounding lower density residential uses through its siting, design and exterior finishings;
- to discourage low density single storey commercial development;
- to discourage auto-oriented commercial uses including drive-through uses;
- to create a distinctive, pedestrian-friendly residential, shopping, office and cultural district that serves the needs of local residents;
- to eliminate conflict between the commercial and residential components and address issues such as loading and garbage.

Commercial Mixed-Use Development Permit Guidelines (DPA 2):

1. ENTRANCES | Ground-level entranceways to all retail and office-commercial buildings should be designed so as to provide visual interest and diversity along the street level, as well as to adequately signal pedestrians and passing motorists of the entrance location.
2. BUILDING HEIGHT TRANSITIONS | Building height transitions shall be considered to ensure compatibility between multi storey buildings and lower intensity development on adjacent properties. Unfinished side walls are not permitted.
3. COMMERCIAL | Maximize active uses (e.g. shop fronts) along the public interface, including along streets and public thoroughfares.
4. COMMERCIAL | Orient primary retail or commercial unit entrances towards the street/plazas rather than to parking areas.
5. COMMERCIAL | Maximize retail/commercial glazing at the street frontage and avoid overhanging building arcades. Avoid the use of signage or other images on windows to prevent visual connections to the street.
6. COMMERCIAL | Provide a finer grain of detailing of ground level frontages to add interest and character and opportunities for outdoor patios.
7. SHELTER | Continuous weather protection over main entrances and over building facades shall be encouraged.
8. SHADING | Building siting and internal spaces should be designed to promote natural ventilation, reducing reliance on mechanical means. Shading devices should be used to control solar heat gain during summer months while permitting sunlight into living space during the cold months.
9. ROOFS | Stepped roof lines that scale buildings from major to minor elements are strongly encouraged.
10. PEDESTRIAN | All Commercial Mixed-Use developments shall be designed with consideration to integration with the Anmore South Pedestrian Network and creating a positive pedestrian realm.
11. ACCESSIBILITY | Wherever possible, all outdoor public areas of Commercial Mixed-Use developments are to be accessible to persons with physical disabilities.
12. PARKING | Convenience surface parking shall be provided to ensure ease of access and support commercial viability.
13. LOADING | Loading areas for commercial uses should be provided behind buildings.
14. SCREENING | Garbage and recycling containers, utility boxes, fans, vents, and unenclosed outdoor storage areas should be screened from public view and located for convenient access by service vehicles.

15. GARBAGE | Garbage and recycling containers should be wildlife resistant.
16. SIGNAGE | Commercial signage shall complement the design of buildings and structures and be grouped, where possible, in multiple development sites. The use of natural materials is encouraged. A consistent signage approach is encouraged.
17. SIGNAGE | The size, location and design of freestanding signage shall be architecturally integrated with the overall design of the buildings and landscaping. The design of fascia signs containing individual signage shall be integrated into the design of the building.
18. SIGNAGE | No roof top signs shall be permitted. Multi-tenant buildings shall provide combined tenant signage.
19. LIGHTING | The use of indirect and/or accent lighting on signage is encouraged.
20. DARK SKY LIGHTING | Lighting should be designed, both outside and inside developments, in accordance with Dark Sky guidelines to improve safety, minimize glare and preserve the ambiance of the night sky. The use of video signage is not permitted.
21. LIGHTING | All new, replacement and upgraded exterior lighting in existing and proposed developments shall use Full-Cut Off/Flat Lens (FCO/FL) luminaires as required for roads, parking, loading and pedestrian areas. Exterior building lighting will also be required to use FCO lighting fixtures.

8.4 DPA 3: NATURAL ENVIRONMENT

Purpose of Designation Category

Pursuant to subsection 488 (1) (a) of the Local Government Act, the purpose of this designation is to protect the natural environment, its ecosystems and biological diversity.

Justification

The Natural Environment DPA is applicable to all properties in the Neighbourhood Plan that may be developed.

The Village of Anmore has extensive natural areas that include significant forested areas, streams, riparian areas, wetlands and steep hillsides and rock features. These areas are ecosystems that provide many functions necessary for the health and well-being of fish, wildlife and people.

The intent of this DPA is to guide development to avoid negative effects on environmentally sensitive and significant areas, habitat, water quality, biodiversity, air quality, greenhouse gas emissions, watercourse maintenance, outdoor recreation opportunities, and other tangible and intangible benefits of natural areas.

To ensure the protection of riparian areas the Anmore OCP outlines in Schedule F the Watercourse Protection DPA. The guidelines for the Watercourse Protection DPA, consistent with the requirements of the Province's Riparian Areas Regulation (RAR), are to be contained in the Village's Zoning Bylaw. The following guidelines are intended to be in addition those outlined in the OCP to guide development outside the watercourse zones.

Natural Environment Development Permit Guidelines (DPA 3):

1. HABITAT | Design and implement artificial habitat features, such as nesting boxes or spawning beds, where natural features are not present or practical, and as recommended by a Qualified Environmental Professional (QEP).
2. HABITAT | Design roadways and connections to mitigate habitat fragmentation, disruption and impedance to wildlife travel, with a preference for open-bottom culverts or clear-span bridges for stream crossings.
3. HABITAT | Prevent ecosystem fragmentation by preserving natural area connectivity.
4. INFILTRATION | Limit impervious surfaces to improve stormwater absorption by using permeable materials and techniques, including permeable pavers and structural grass fields.
5. LAYOUT | Limit encroachment into adjacent environmentally sensitive areas by adjusting building construction and site layouts.
6. LAYOUT | Locate development on portions of the site that are least environmentally sensitive by retaining a QEP to complete a bio-inventory prior to site design in accordance with industry standard best practices, including Develop with Care (2014).
7. LIGHT | Design development to limit ambient light pollution and reduce impacts on those habitat areas reliant on darkness.
8. LIGHT | Develop phase-specific plans to comply with Dark Sky policies.
9. MITIGATION | Implement QEP recommendations approved by the Village of Anmore, including environmental mitigation measures.
10. MONITORING | Require environmental monitoring by a QEP during construction adjacent to or within a watercourse or wetland.
11. RAINWATER | Incorporate rainwater Best Management Practices into development so as to mimic pre-development creek flows and groundwater infiltration.
12. RIPARIAN | Enhance and, where feasible, restore watercourses that are compromised by riparian invasive plants to improve watercourse quality.
13. SEDIMENT + EROSION | Install both temporary and permanent sediment/pollutant containment and erosion control measures for any development.

14. SEDIMENT + EROSION | Require phased and site-specific Erosion and Sediment Control Plans to be prepared for developments by Professional Engineers, including requirements for discharge water quality and monitoring during construction with reports provided to the Village.
15. VEGETATION | Create continuous vegetated corridors, wherever possible, through planting or linking tree retention areas from within the development to other retained natural habitats.
16. VEGETATION | Mitigate impacts on mature trees or require replacement planting as recommended by a QEP. Replacement plantings should be native to the Coastal Western Hemlock Biogeoclimatic Zone (preferably drought-tolerant species). Ensure the root systems are protected from disturbance.
17. VEGETATION | Prevent disturbance of animal and bird nesting sites and breeding areas by preserving adequate animal habitat through the retention of appropriate trees and vegetation and by timing construction activity to avoid bird breeding windows as directed by a QEP.
18. VEGETATION | Retain existing vegetation across developing neighbourhoods where practical to ensure the health and integrity of the urban forest and topography.
19. VEGETATION | Revegetate unavoidable interruptions along ridgelines with timely landscape plantings in clusters to mimic the natural environment.
20. VEGETATION | Require a Vegetation Restoration Plan to be designed and implemented under the supervision of a QEP or Registered Landscape Architect for areas of vegetation impact.
21. VEGETATION | Collaborate with local stewardship organizations (e.g., Mossom Creek Watershed Society) on appropriate native planting species lists.
22. DESIGN | Consider Bird-Friendly Design into building and landscaping.
23. DESIGN | Design and incorporate access deterrents for Environmentally Sensitive Areas.
24. DESIGN | Require incorporation of landscape plants which benefit birds and invertebrate pollinators.
25. MITIGATION | Implement mitigation recommendations included in the AquaTerra Environmental Impact Assessment report (Appendix C).
26. ISMP | Require implementation of an Invasive Species Management Plan prepared by a QEP.

8.5 DPA 4: HILLSIDE DEVELOPMENT

Purpose of Designation Category

Pursuant to subsection 488 (1) (b) of the Local Government Act, the purpose of this designation is to protect development from hazardous conditions.

Justification

The Hillside Development DPA is applicable to all properties in the Neighbourhood Plan that may be developed.

The Anmore South neighbourhood includes both gentle and sloped lands, with the steepest slopes primarily associated with the incised ravine systems of Doctors and Schoolhouse Creeks. This DPA is applied to protect development from potential hazards associated with slope stability as well as to ensure neighbourhood development that works with the existing terrain to maintain landscape character.

Hillside Development Permit Guidelines (DPA4):

The following common guidelines should be applied for all development in the Hillside Development Permit Area:

1. GEOTECH | Require a geotechnical report to be completed for all Hillside Development Permit Applications to establish the feasibility of development in a safe manner.
2. HILLSIDE | Retain and, where possible, enhance significant natural scenic features, such as gullies, rock outcrops and knolls.
3. HILLSIDE | Consider use of single loaded streets or split lanes with narrow roads to avoid scenic features and reduce grading.
4. CLEARING | Limit vegetation clearing, stripping of top-soils, and bulk grading, to the extent required by development phase.
5. GRADING | Recognize the existing topographic conditions and locate development and infrastructure - including building layouts and roads - in a manner that manages the need for significant cuts and fills.
6. GRADING | Where practical, avoid large cuts/fills to create 'build-able lots' or flat yards.
7. GRADING | Avoid site and lot grading that negatively impacts designated visual impact buffers.

8. GRADING | Situate manufactured slopes behind buildings.
9. RETAINING | Where possible, design final lot grades to mimic the natural slope thereby limiting use of retaining walls.
10. RETAINING | Utilize stepped building foundations and terraced retaining walls to manage lot grade changes.
11. RETAINING | Integrate retaining structures with the onsite architectural character to reduce slope disturbance.
12. RETAINING | Limit the total height of new building lot retaining walls across the property.
13. RETAINING | Design road, driveway, and retaining walls to conform to the natural terrain, where possible. Retaining walls to remain in private ownership where practical.
14. MITIGATE | Re-vegetate recontoured slopes promptly using native plant palette.
15. BUILDINGS | Design buildings to limit the visual impacts associated with development along ridgelines and edge of steep ravines and hillsides greater than 30% slope.
16. BUILDINGS | Set back buildings, retaining walls and fences from the edge of natural features, such as ravines, cliffs, rock knolls or outcrops.
17. BUILDINGS | Ensure building and retaining design, color and finish complement natural features and terrain.
18. MITIGATE | Incorporate scenic natural features into the subdivision design.
19. MITIGATE | Limit individual lot impacts through the use of extensive screening with mature landscape materials, greater rear yard setbacks, stepped building massing, planted hedges in place of fencing.
20. MITIGATE | Avoid uniform retaining walls or mitigate with mature landscaping.

9



Figures + Schedules

Figure 1
CONTEXT PLAN

LEGEND MAY 2025

- Site Boundary
61.14 ha / 151.08 ac
- Village of Anmore Boundary
- Property Line
- Surveyed Watercourse

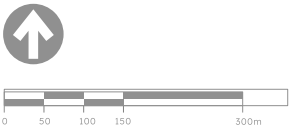


Figure 2
AERIAL ANALYSIS

LEGEND MAY 2025

- Site Boundary 61.14 ha / 151.08 ac
- Municipality Boundary
- Property Line
- Surveyed Watercourse

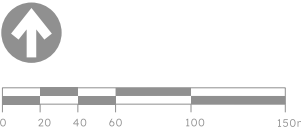
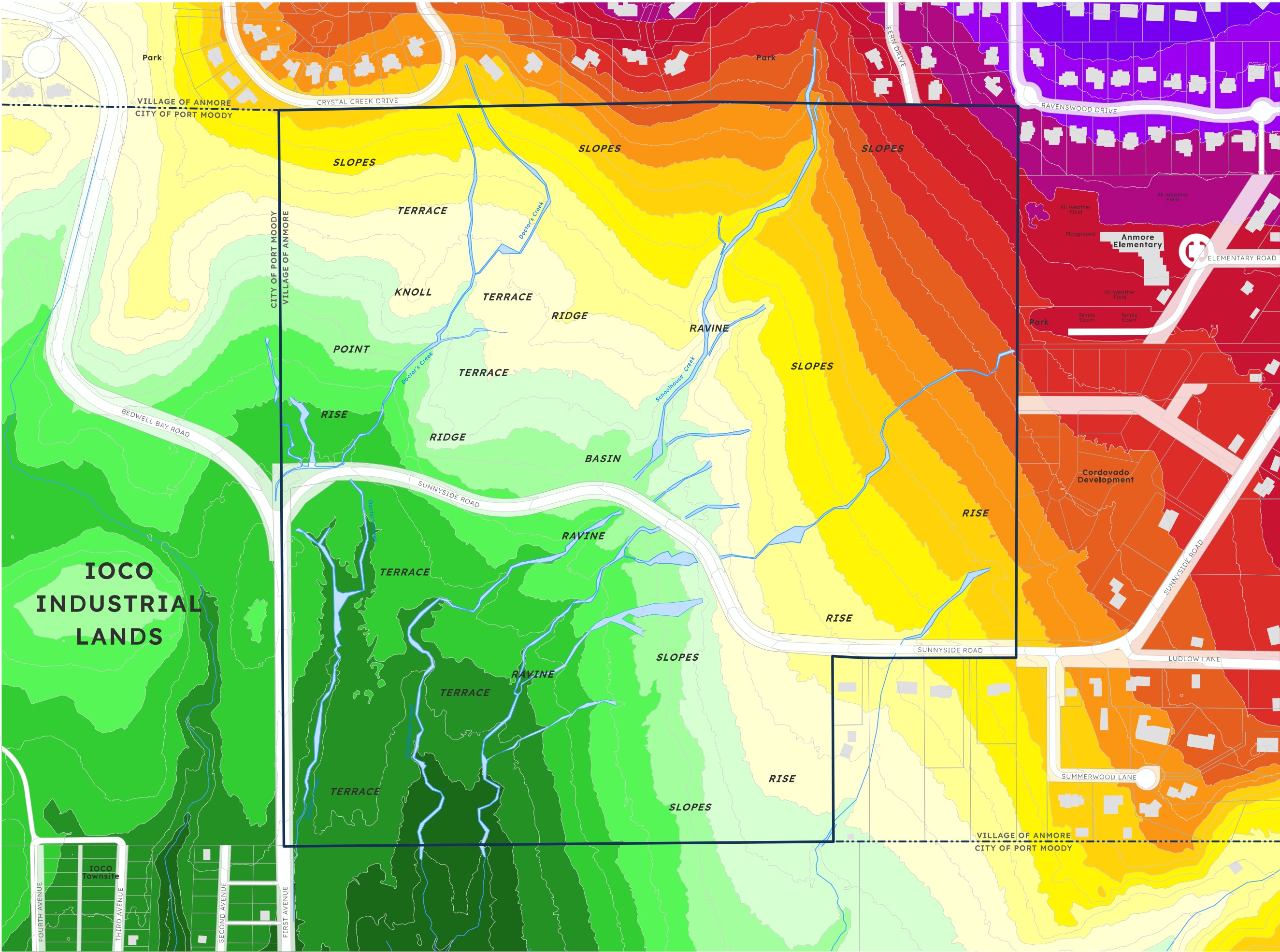


Figure 3
LANDFORM ANALYSIS



LEGEND MAY 2025

- Site Boundary 61.14 ha / 151.08 ac
- Municipality Boundary
- Property Line
- Surveyed Watercourse
- 5m Contour
- Existing Structures

- 180m - 190m
- 170m - 180m
- 160m - 170m
- 150m - 160m
- 140m - 150m
- 130m - 140m
- 120m - 130m
- 110m - 120m
- 100m - 110m
- 90m - 100m
- 80m - 90m
- 70m - 80m
- 60m - 70m
- 50m - 60m
- 40m - 50m
- 30m - 40m
- 20m - 30m
- 10m - 20m

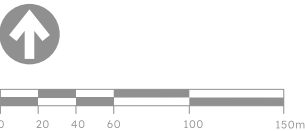
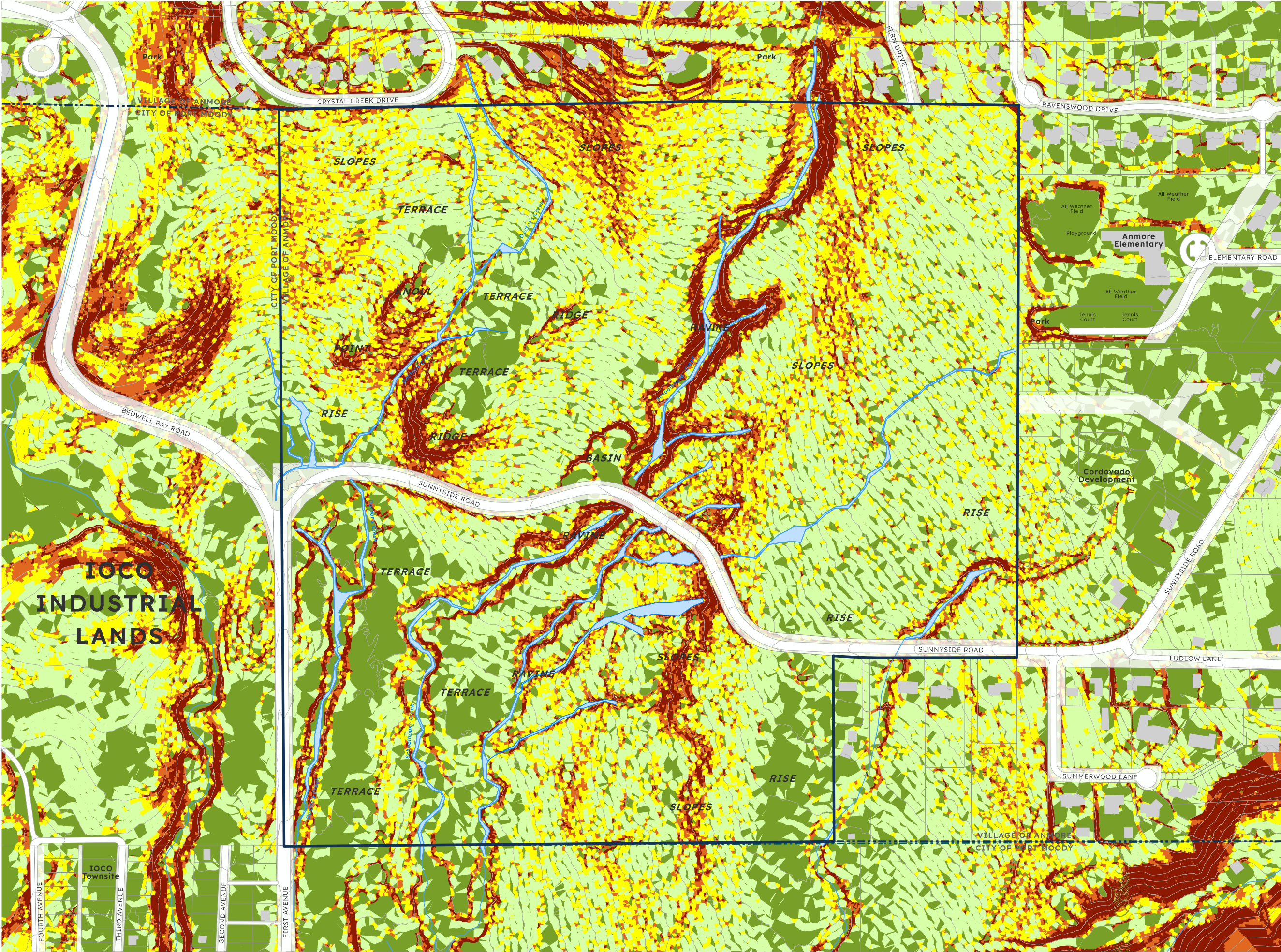


Figure 4
SLOPE ANALYSIS



LEGEND MAY 2025

- Site Boundary 61.14 ha / 151.08 ac
- Municipality Boundary
- Property Line
- Surveyed Watercourse
- 5m Contour
- Existing Structures

- SLOPE ANALYSIS**
- 0-10% Slope | Flat
 - 10-20% Slope | Gentle
 - 20-30% Slope | Hillside
 - 30-40% Slope | Steep
 - +40% Slope | Extreme

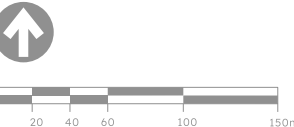
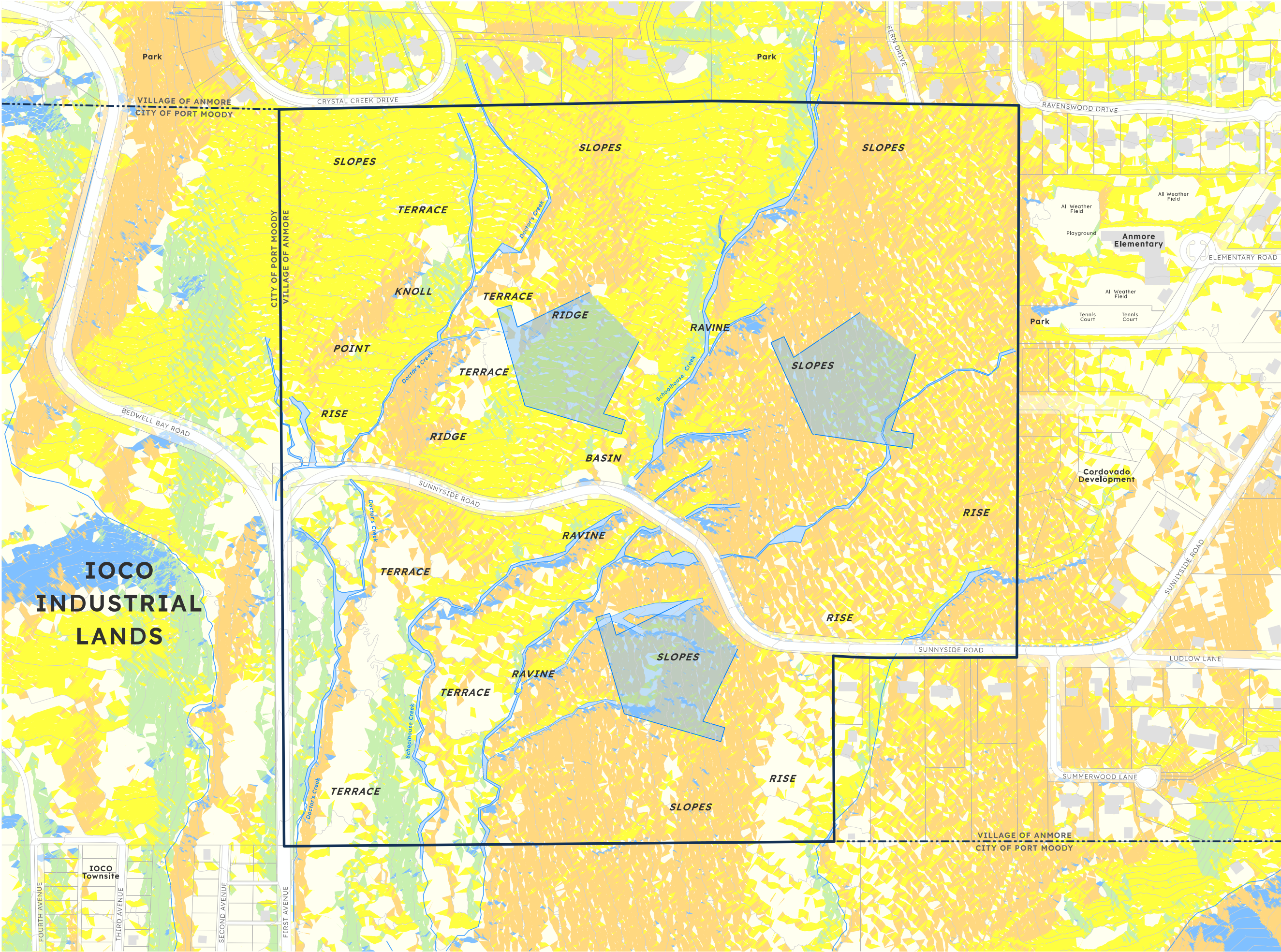


Figure 5
ASPECT ANALYSIS



LEGEND MAY 2025

- Site Boundary 61.14 ha / 151.08 ac
- Municipality Boundary
- Property Line
- Surveyed Watercourse
- 5m Contour
- Existing Structures
- Significant SW Views towards Burnaby Mountain

0-10% Slope | Flat

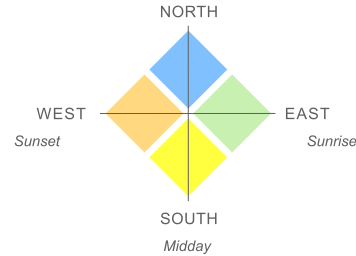


Figure 6:
ILLUSTRATIVE
CONCEPT PLAN

LEGEND MAY 2025

- Site Boundary 61.14 ha | 151.08 ac
- Municipality Boundary
- Property Line
- Surveyed Top of Bank
- Potential Offsite Field Upgrade

- Civic Community Centre
- Single Family / Duplex
- Townhome
- Apartment
- Mixed-Use

Note: Plan is for Illustrative Purposes Only.

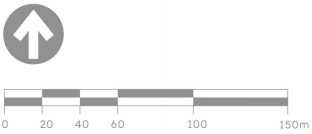


Figure 7A
COLLECTOR
STREET CROSS SECTION

LEGEND MAY 2025

- COLLECTOR ROAD FEATURES**
- 1 Multi-Use Pathway
 - 2 Public Sidewalk
 - 3 Street Lighting - As Required
 - 4 Street Trees
 - 5 Road Grade Parking Pocket
 - 6 Bioswale

- COLLECTOR ROAD SPECS**
- Speed | 40-50 km/h
 - ROW | 20.0m
 - Lane | 2 - 3.5m
 - Carriage | 7.0m
 - Curbing | Barrier - Safety
 - Parking | Road Grade Parking Pocket
 - Boulevard | 2.4m / 2.2m Boulevard with curb
 - MUP | 3.0m MUP + 1.8m Sidewalk
 - Cycle | MUP Off-Road
 - Trees | Boulevard, Both Sides
 - Utilities | Under Boulevard MUP
 - Offset Boulevard | 0.5m from ROW

Note: The proposed Cross Section is conceptual and will be refined through detailed design.

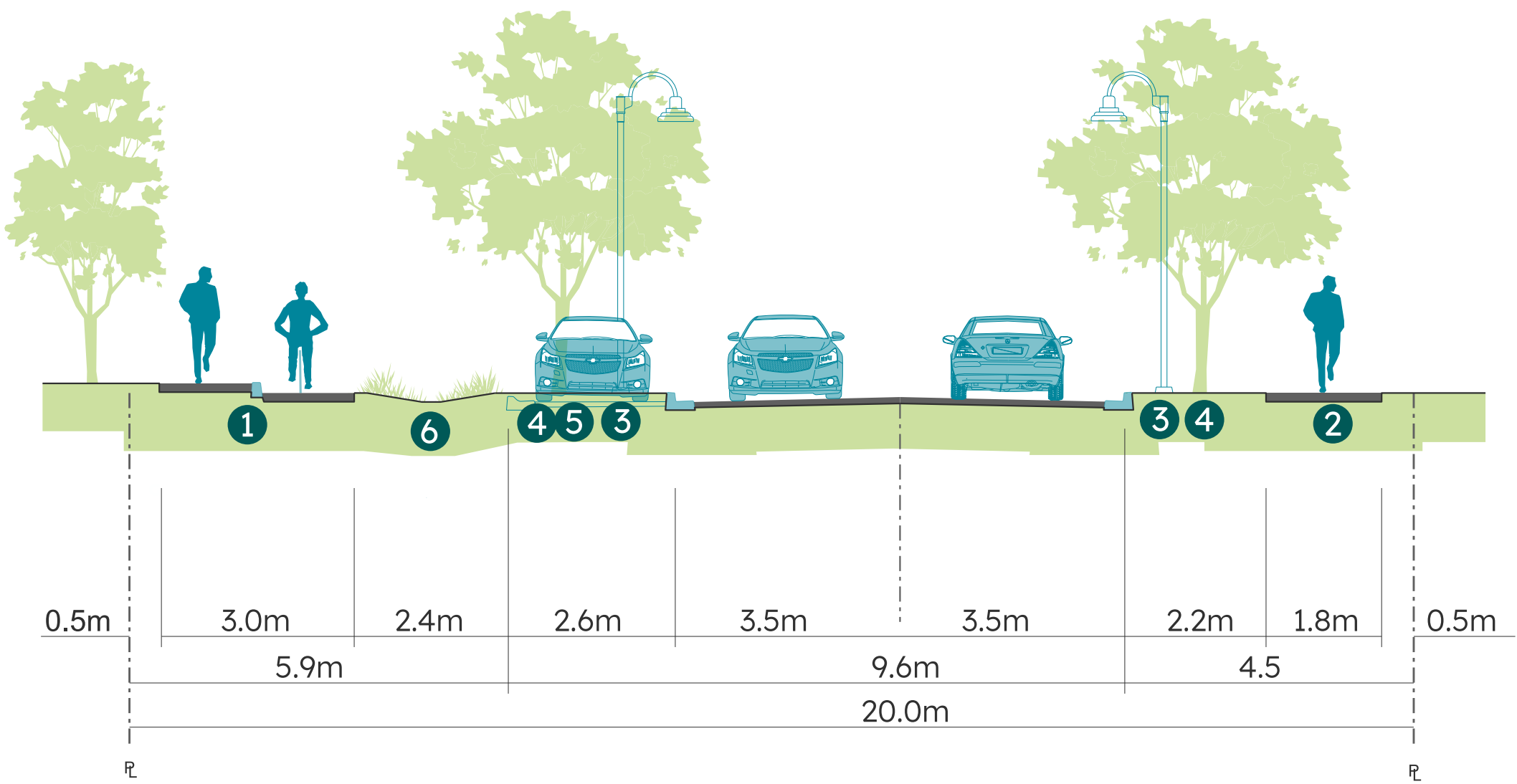
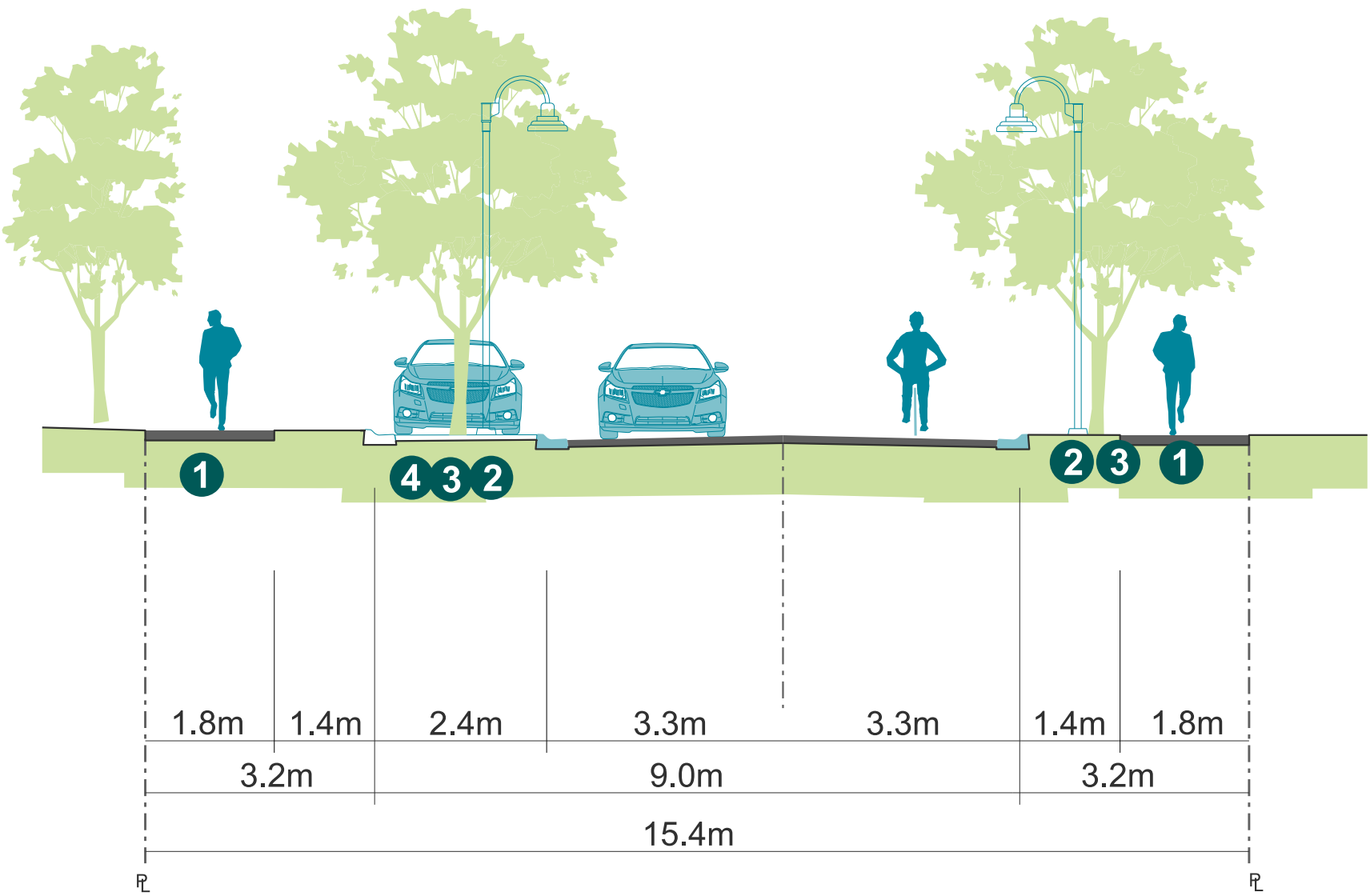


Figure 7B
LOCAL
STREET CROSS SECTION



LEGEND MAY 2025

- LOCAL ROAD FEATURES**
- 1 Public Sidewalk
 - 2 Street Lighting - As Required
 - 3 Street Trees
 - 4 Above Road Parking Pocket | If Required

- LOCAL ROAD SPECS**
- Speed | 30-40 km/h
 - ROW | 15.4m
 - Lane | 2 - 3.3m
 - Carriage | 9.0m
 - Curbing | Rollover
 - Parking | Above Grade Parking
 - Boulevard | 1.4m Boulevard with curb
 - MUP | 2 x 1.8m Sidewalk
 - Cycle | on-Road
 - Trees | Boulevard, Both Sides
 - Utilities | Under Boulevard

Note: The proposed Cross Section is conceptual and will be refined through detailed design.

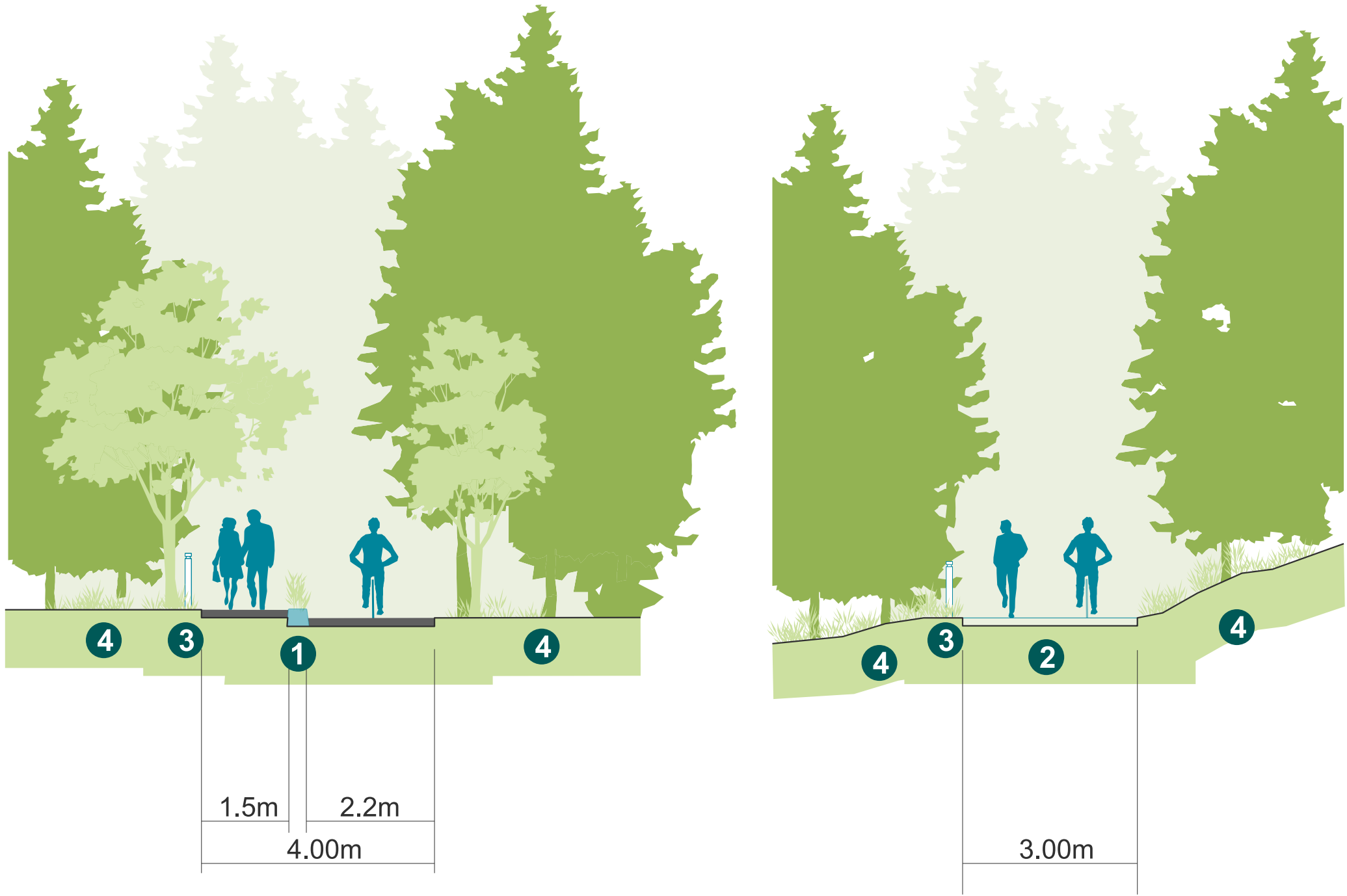
Figure 7C
MULTI-USE PATHWAY
CROSS SECTION

LEGEND MAY 2025

MULTI-USE PATHWAY FEATURES

- 1 Grade Separated Multi-Use Pathway
- 2 Packed Gravel Multi-Use Trail
- 3 Bollard Lighting
- 4 Retained + New Planted Landscape

Note: The proposed Cross Sections are conceptual and will be refined through detailed design.



Schedule A
LAND USE PLAN

LEGEND MAY 2025

- Site Boundary 61.14 ha | 151.08 ac
- Municipality Boundary
- Property Line
- Surveyed Top of Bank

LAND USE	
3%	Ground Oriented 1.84 ha 4.55 ac
29%	Multi-Family Townhome 17.88 ha 44.18 ac
7%	Multi-Family Apartment 3.90 ha 9.64 ac
2%	Mixed Use 1.50 ha 3.70 ac
1%	Civic Community Centre 0.49 ha 1.21 ac
1%	Civic Development Reserve 0.48 ha 1.19 ac
7%	Neighbourhood Park 4.34 ha 10.72 ac
35%	Conservation + Recreation 21.35 ha 52.76 ac
15%	Road ROW 9.36 ha 23.13 ac
100%	61.14 ha 151.08 ac



0 20 40 60 100 150m



Schedule B
PARK + TRAIL
NETWORK PLAN

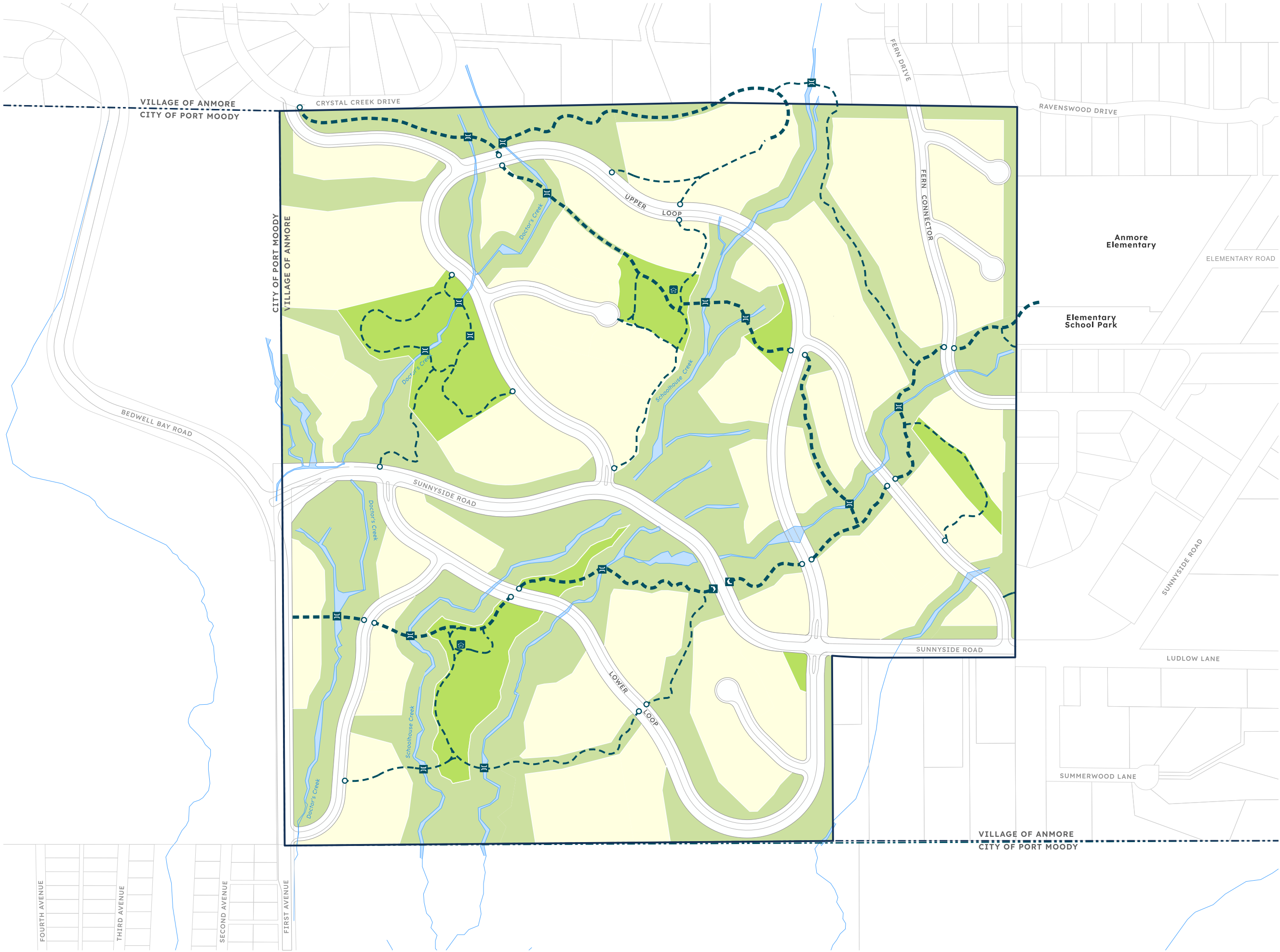
LEGEND MAY 2025

- Site Boundary 61.14 ha | 151.08 ac
- Municipality Boundary
- Property Line
- Surveyed Top of Bank

PARK + TRAIL NETWORK

43%	Development Areas 26.09 ha 64.47 ac
7%	Neighbourhood Park 4.34 ha 10.72 ac
35%	Conservation + Recreation 21.35 ha 52.76 ac
15%	Road ROW 9.36 ha 23.13 ac
100%	61.14 ha 151.08 ac

- 4m | Multi-Use Greenway
- 3.0 m | Multi-Use Trail
- Potential Trailhead
- Potential Stream Crossing
- Underpass
- Pavilion



Schedule C
ENVIRONMENTAL
NETWORK PLAN

LEGEND MAY 2025

- Site Boundary
- Municipality Boundary
- Property Line
- Surveyed Top of Bank

ENVIRONMENTAL NETWORK

43%	Development Lands 26.09 ha 64.47 ac	} 58%
15%	Road ROW 9.36 ha 23.13 ac	
19%	Retained Riparian Area 11.28 ha 27.87 ac	} 42%
7%	Neighbourhood Park 4.34 ha 10.72 ac	
16%	Conservation + Recreation (Non-Riparian) 10.07 ha 24.88 ac	
100%	61.14 ha 151.08 ac	100%

- Class A Streams
- Class B Streams
- Class C Streams



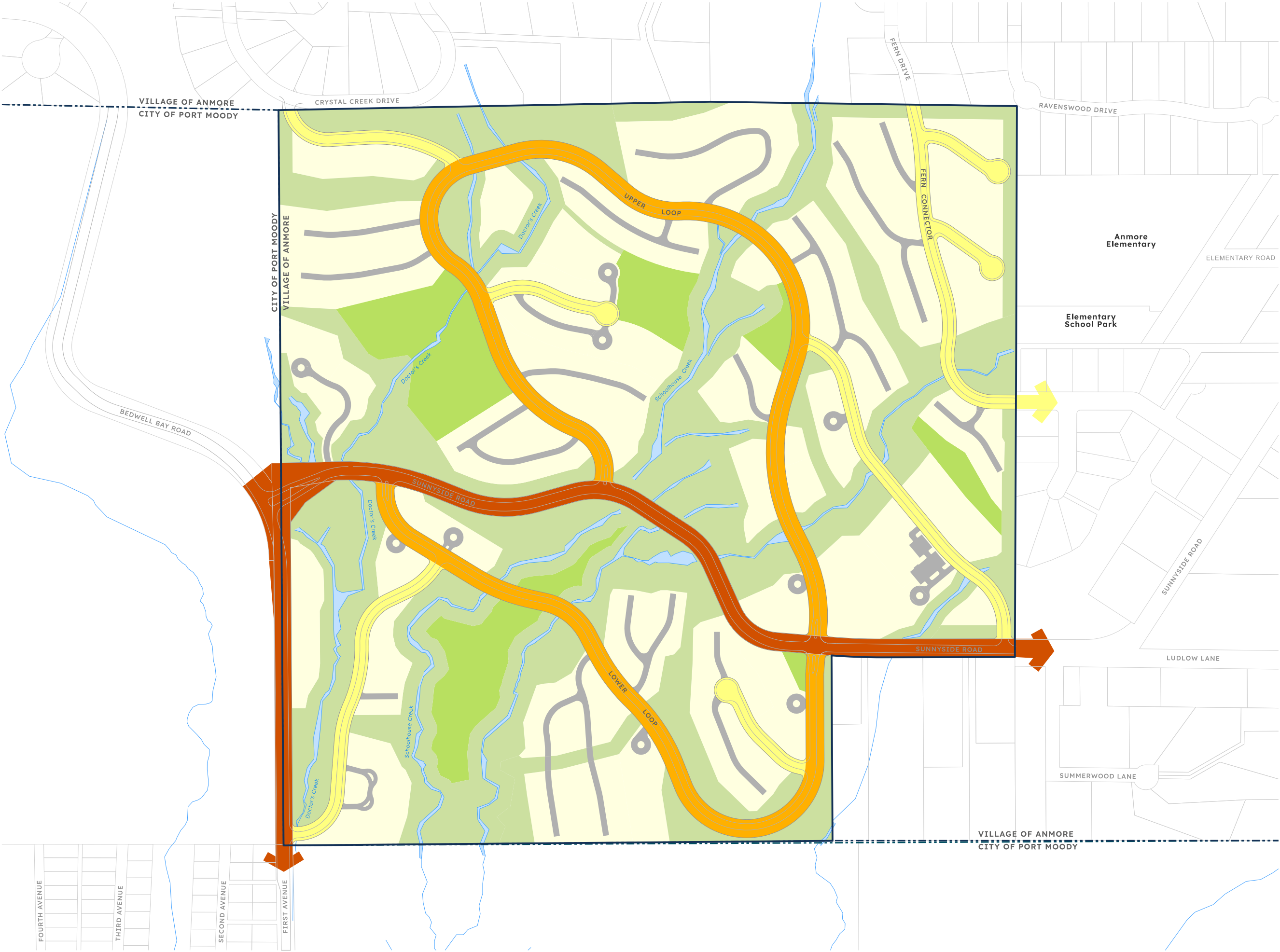
Schedule D
STREET HIERARCHY
PLAN

LEGEND MAY 2025

- Site Boundary 61.14 ha | 151.08 ac
- Municipality Boundary
- Property Line
- Surveyed Top of Bank

EXISTING MAJOR ROAD NETWORK

- Existing Collector | Sunnyside + First Avenue
- Collector
- Local
- Strata, Private Drive + Access



Schedule E
PEDESTRIAN
NETWORK PLAN

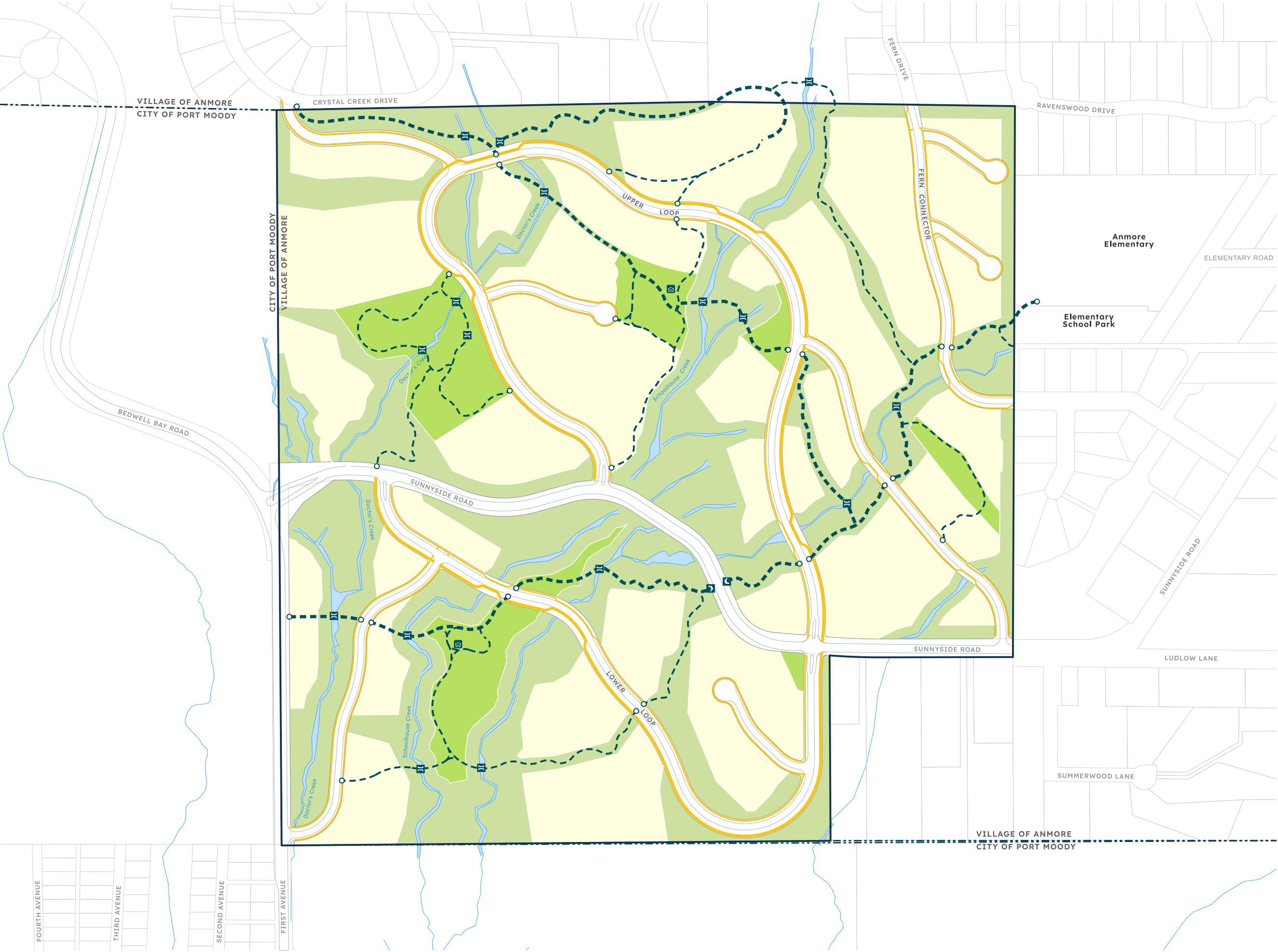
LEGEND MAY 2025

- Site Boundary 61.14 ha | 151.08 ac
- Municipality Boundary
- Property Line
- Surveyed Top of Bank

PARK + TRAIL NETWORK

43%	Development Areas 26.09 ha 64.47 ac
7%	Neighbourhood Park 4.34 ha 10.72 ac
35%	Conservation + Recreation 21.35 ha 52.76 ac
15%	Road ROW 9.36 ha 23.13 ac
100%	61.14 ha 151.08 ac

- 4m | Multi-Use Greenway
- 3.0 m | Multi-Use Trail
- 3.0 m | Multi-Use Sidewalk
- 1.8 m | Sidewalk
- Potential Trailhead
- Potential Stream Crossing
- Underpass
- Pavilion



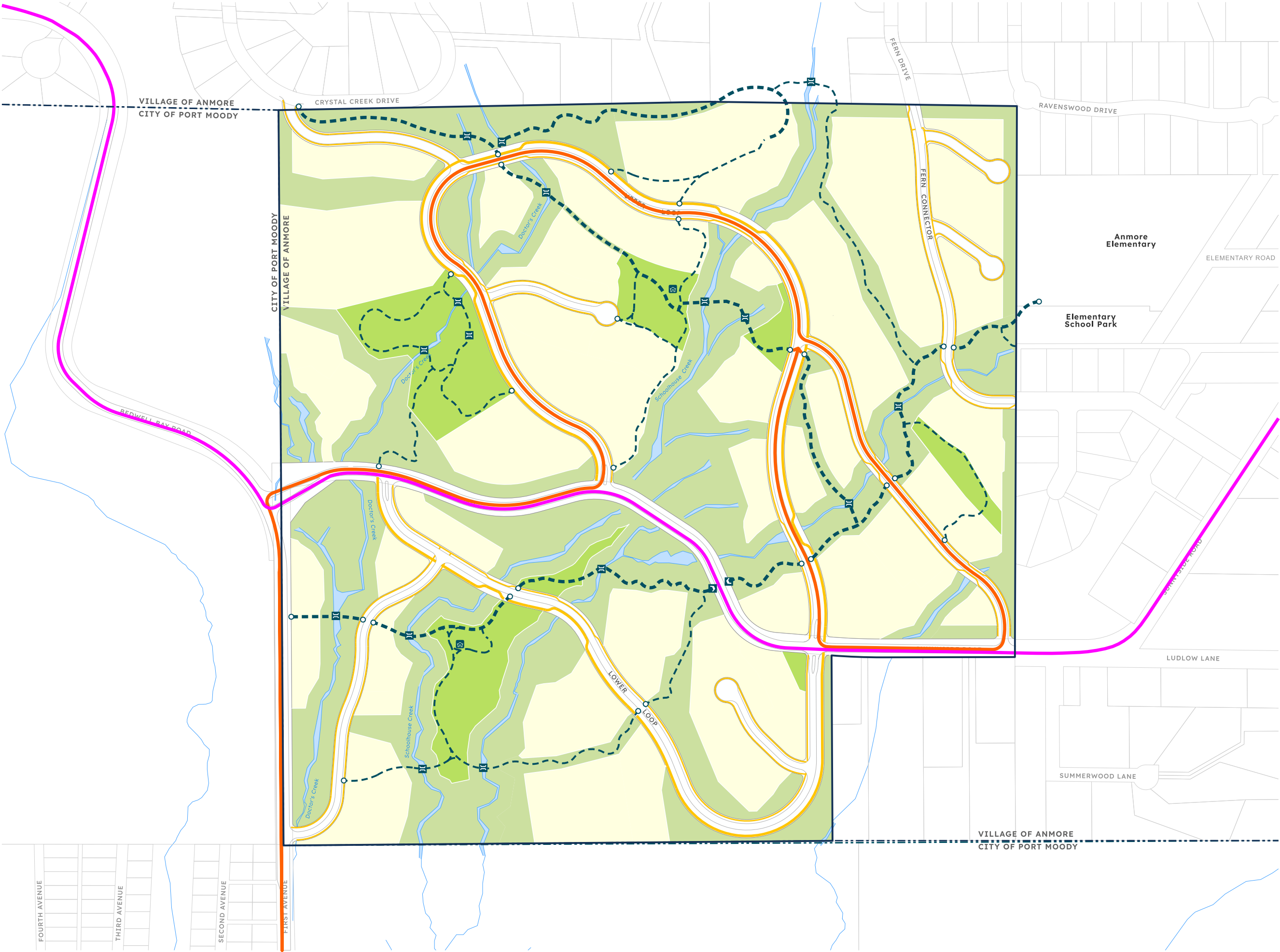
Schedule F
TRANSPORTATION
PLAN

LEGEND MAY 2025

- Site Boundary 61.14 ha | 151.08 ac
- Municipality Boundary
- Property Line
- Surveyed Top of Bank

TRANSPORTATION PLAN

- Existing Route 182
- Proposed Route 181 Extension
- 4m | Multi-Use Greenway
- 3.0 m | Multi-Use Trail
- 3.0 m | Multi-Use Sidewalk
- 1.8 m | Sidewalk
- Potential Trailhead
- Potential Stream Crossing
- Underpass
- Parking Area
- Pavilion



Schedule G
WATER SERVICING
CONCEPT

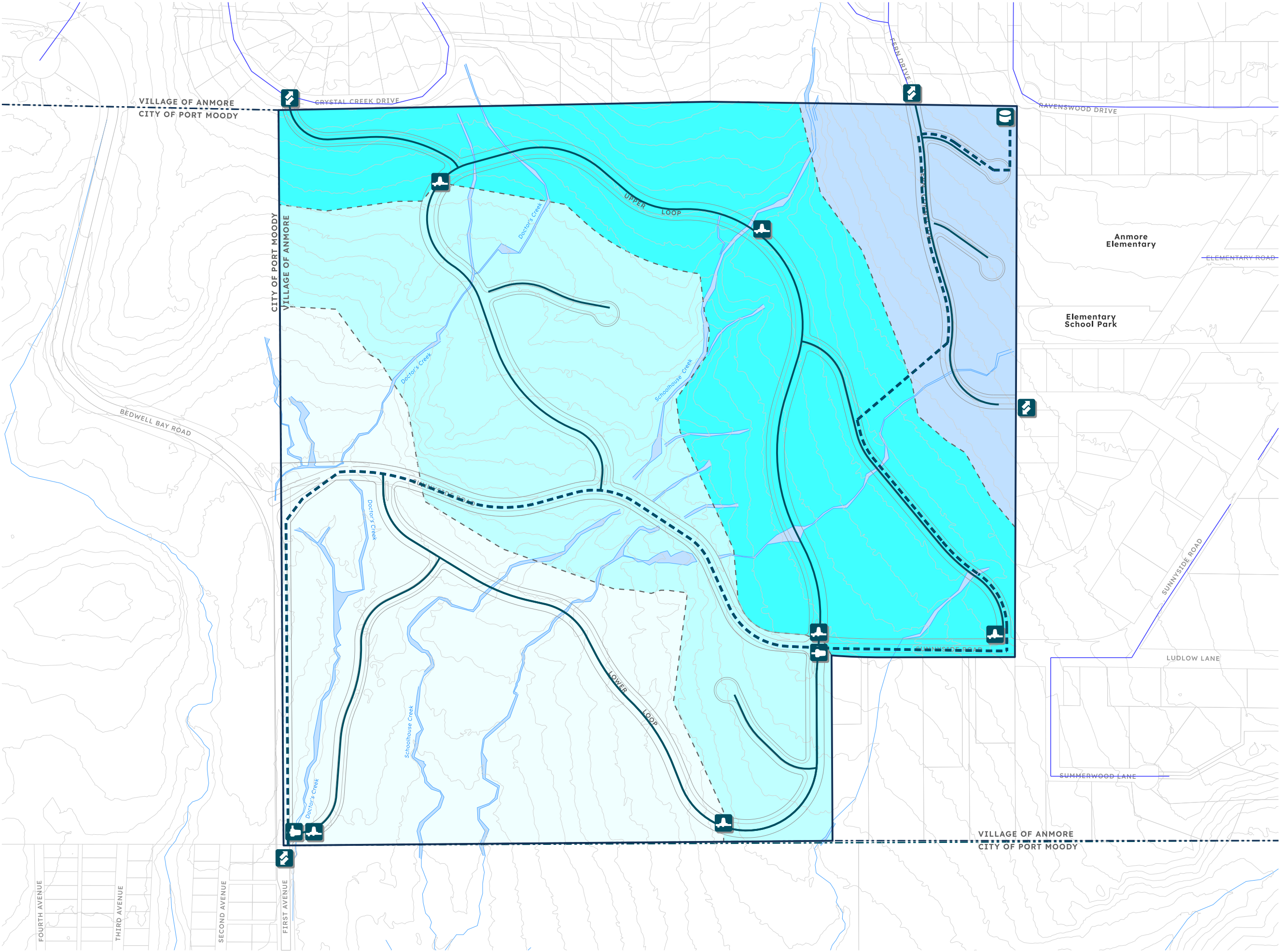
LEGEND MAY 2025

- Site Boundary 61.14 ha | 151.08 ac
- Municipality Boundary
- Property Line
- Surveyed Top of Bank
- 5m Contour

WATER SERVICING

- Pressure Zone Boundary
- 190m Zone
- 150m Zone
- 125m Zone
- 95m Zone
- Existing Distribution Main
- Proposed Distribution Main
- Proposed Feeder Line
- Proposed Reservoir
- Proposed Tie-in Point
- Proposed Pump Station
- Proposed Pressure Reducing Valve

Note: The servicing design is conceptual and subject to change through detailed design



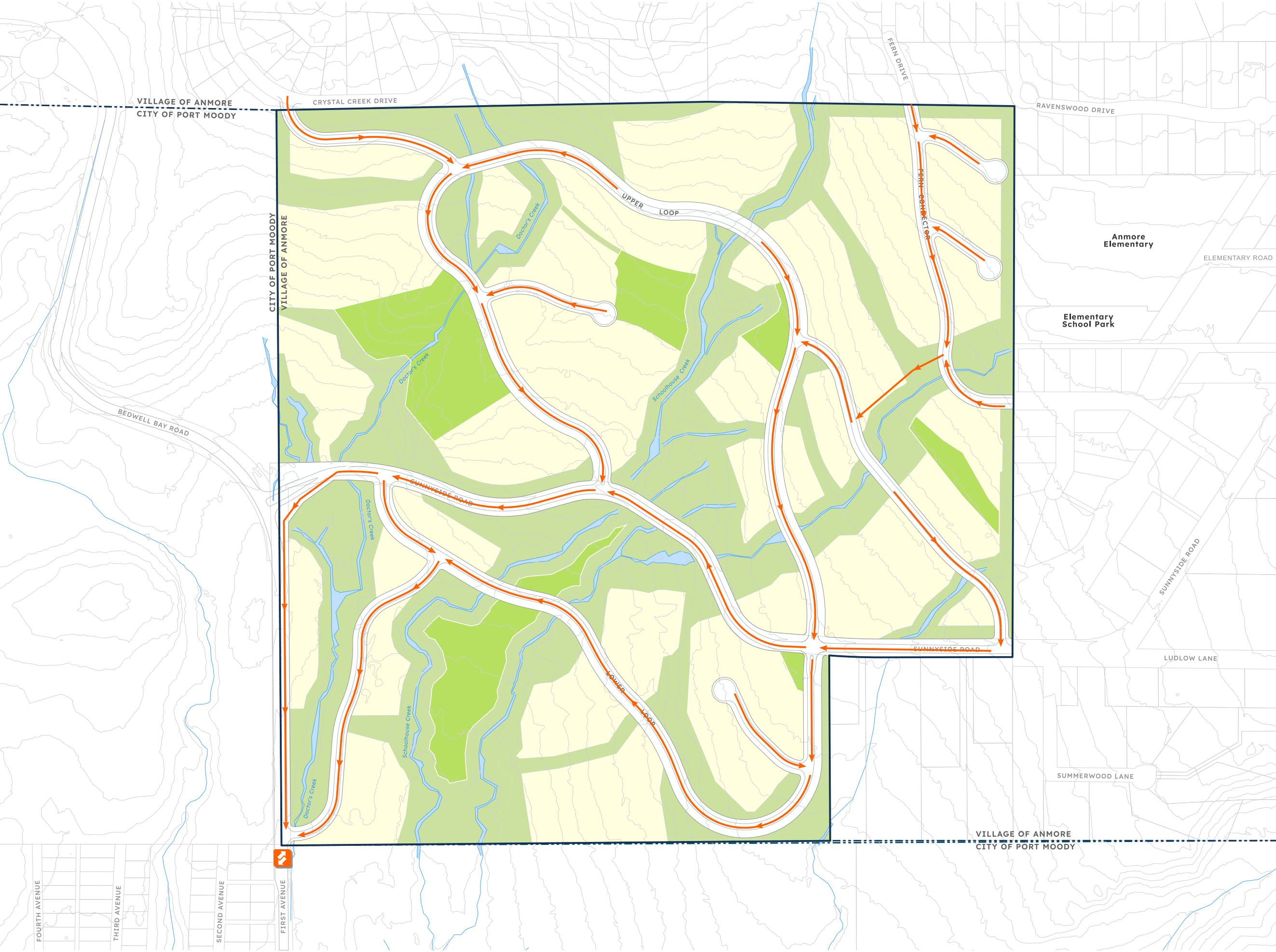
Schedule H
SANITARY SERVICING
CONCEPT

LEGEND MAY 2025

- Site Boundary 61.14 ha | 151.08 ac
- Municipality Boundary
- Property Line
- Surveyed Top of Bank
- 5m Contour

- SANITARY SERVICING**
- Neighbourhood Development Area
 - Neighbourhood Park
 - Conservation + Recreation
 - Sanitary Main
 - Proposed Tie-in Point

Note: The servicing design is conceptual and subject to change through detailed design



Schedule I
DRAINAGE
CONCEPT

LEGEND MAY 2025

- Site Boundary 61.14 ha | 151.08 ac
- Municipality Boundary
- Property Line
- Surveyed Top of Bank
- 5m Contour

DRAINAGE SERVICING

- Neighbourhood Development Area
- Neighbourhood Park
- Conservation + Recreation
- Drainage Main
- Main Outfall
- Culvert

Note: The servicing design is conceptual and subject to change through detailed design



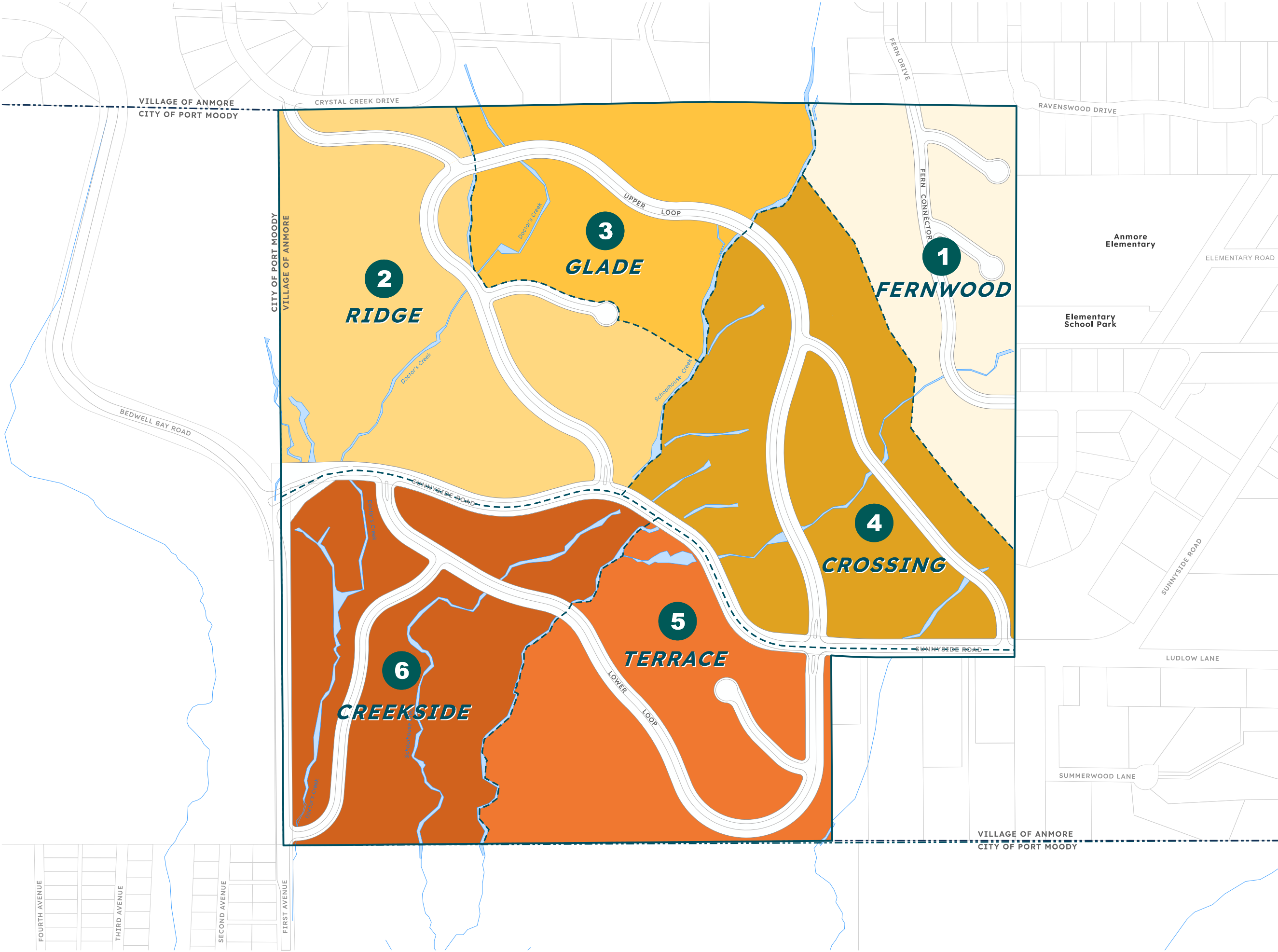
Schedule J
CONCEPTUAL PHASING

LEGEND MAY 2025

- Site Boundary 61.14 ha | 151.08 ac
- Municipality Boundary
- Property Line
- Surveyed Top of Bank

CONCEPTUAL PHASING

- Phase Boundary
- Phase 1
- Phase 2
- Phase 3
- Phase 4
- Phase 5
- Phase 6



0 20 40 60 100 150m



Schedule K
DEVELOPMENT
PERMIT AREAS

LEGEND MAY 2025

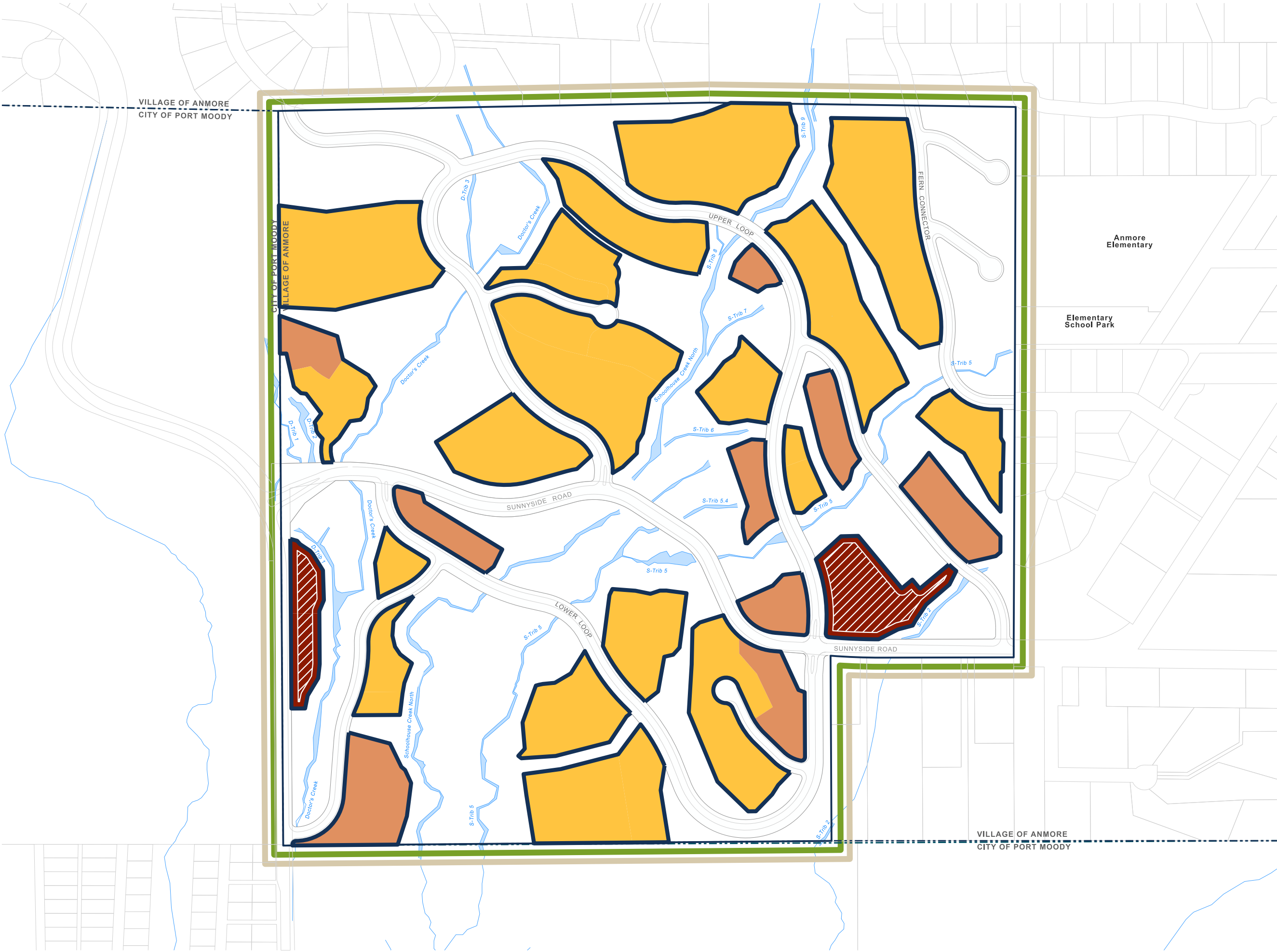
- Site Boundary 61.14 ha / 151.08 ac
- Municipality Boundary
- Property Line
- Surveyed Top of Bank

DEVELOPMENT PERMIT AREAS

- Hillside Development
- Natural Environment
- Multi-Unit Residential
- Commercial Mixed Use

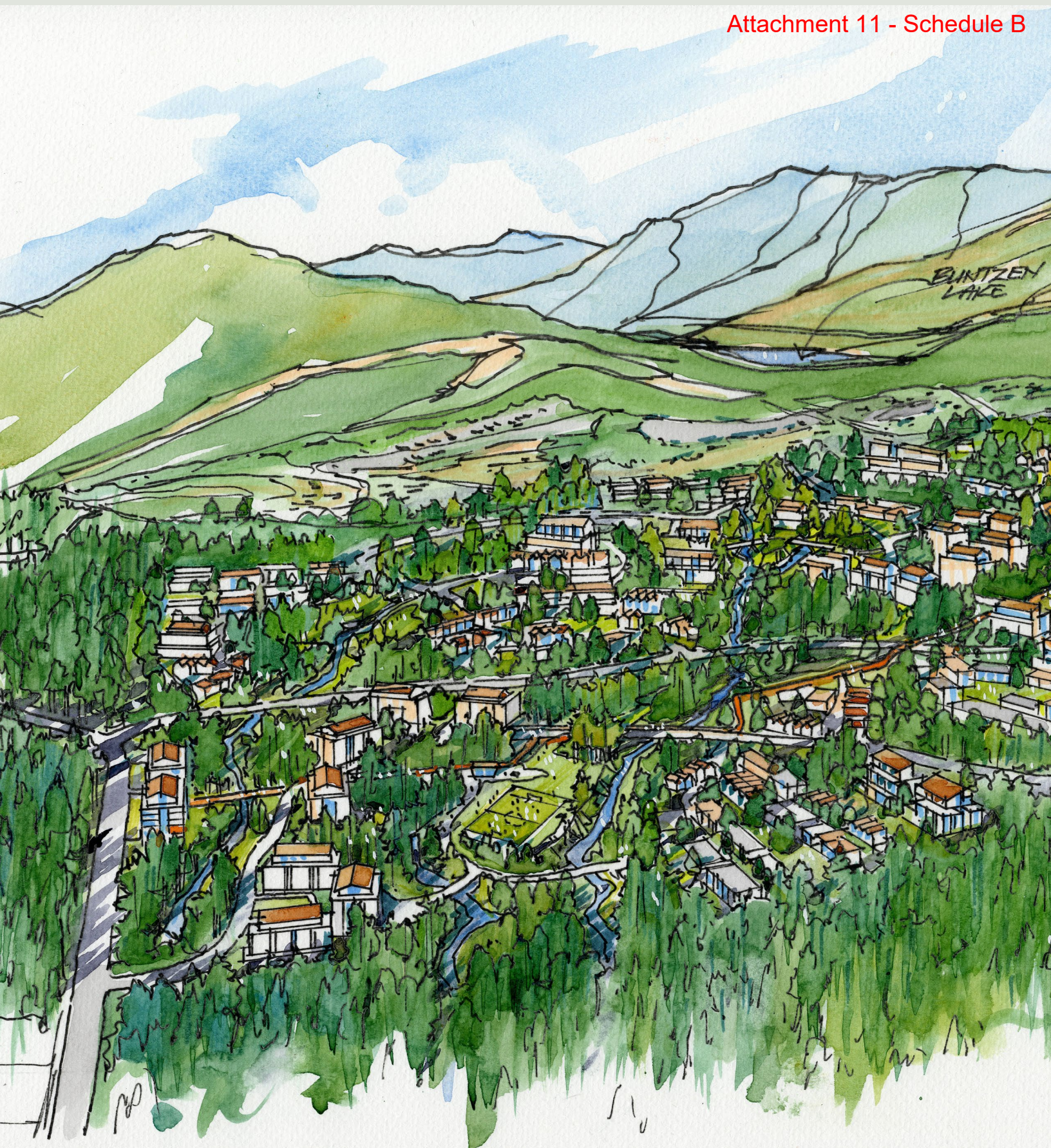
LAND USE

- Multi-Family | Townhome
- Multi-Family | Apartment
- Mixed Use

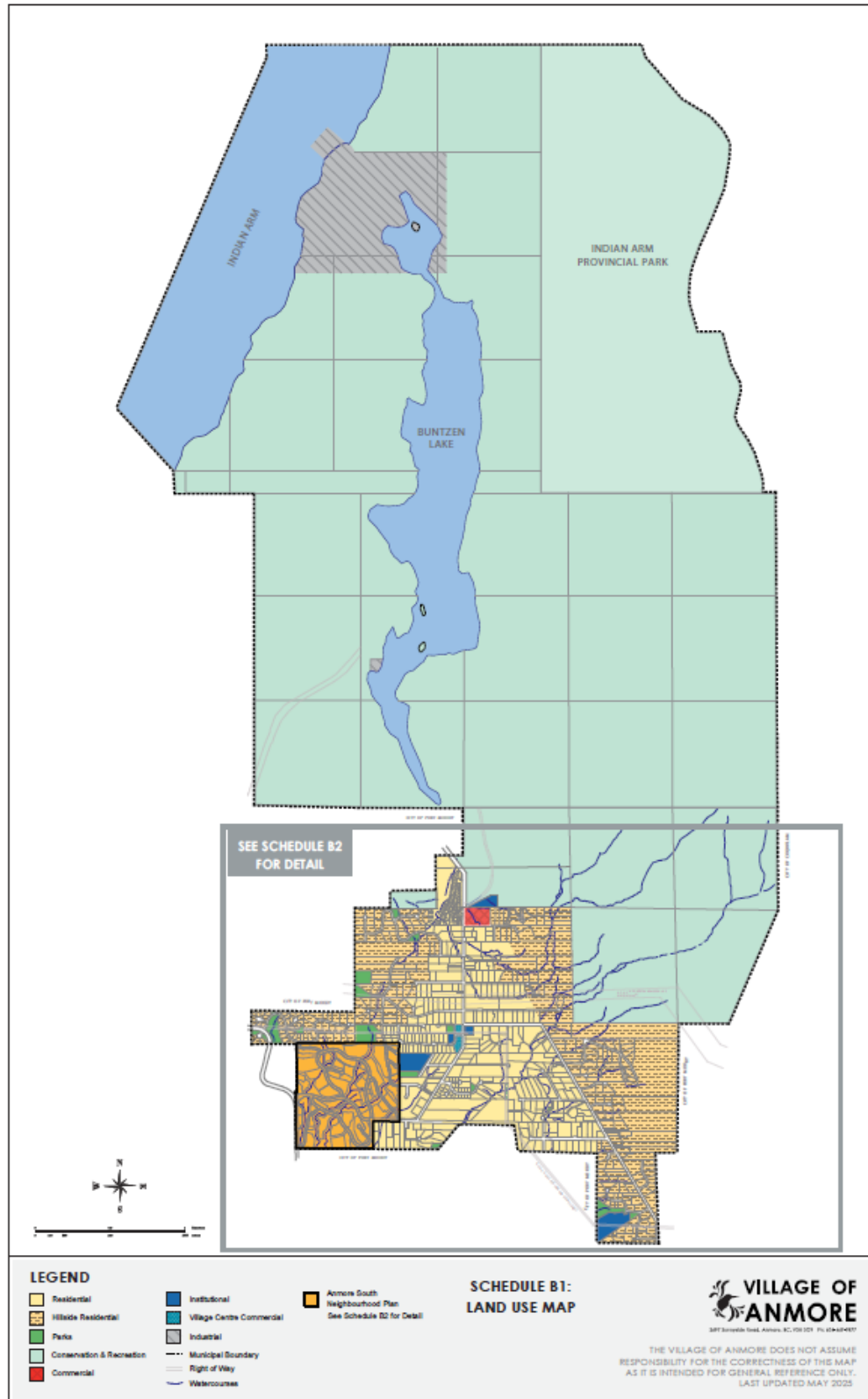


0 20 40 60 100 150m

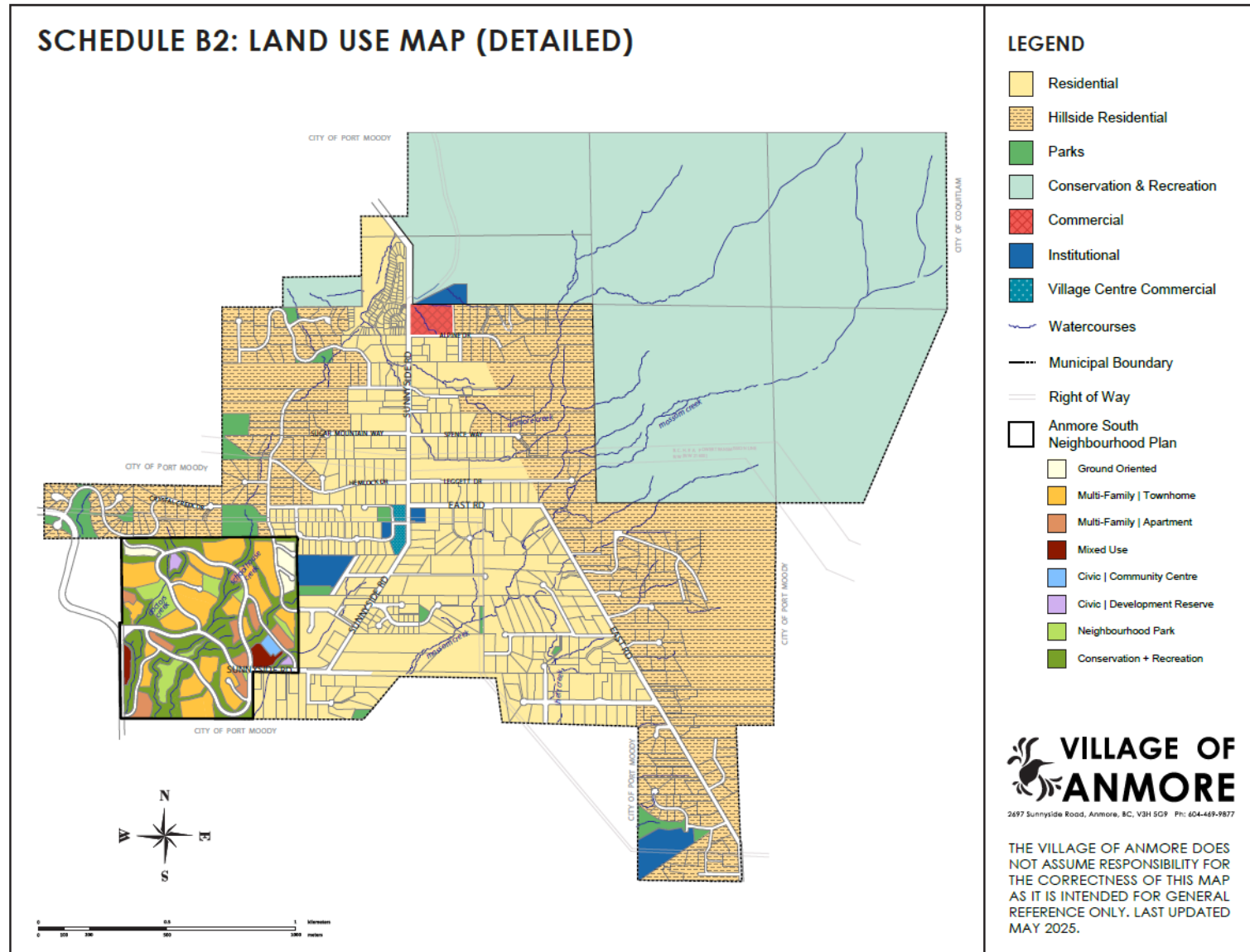




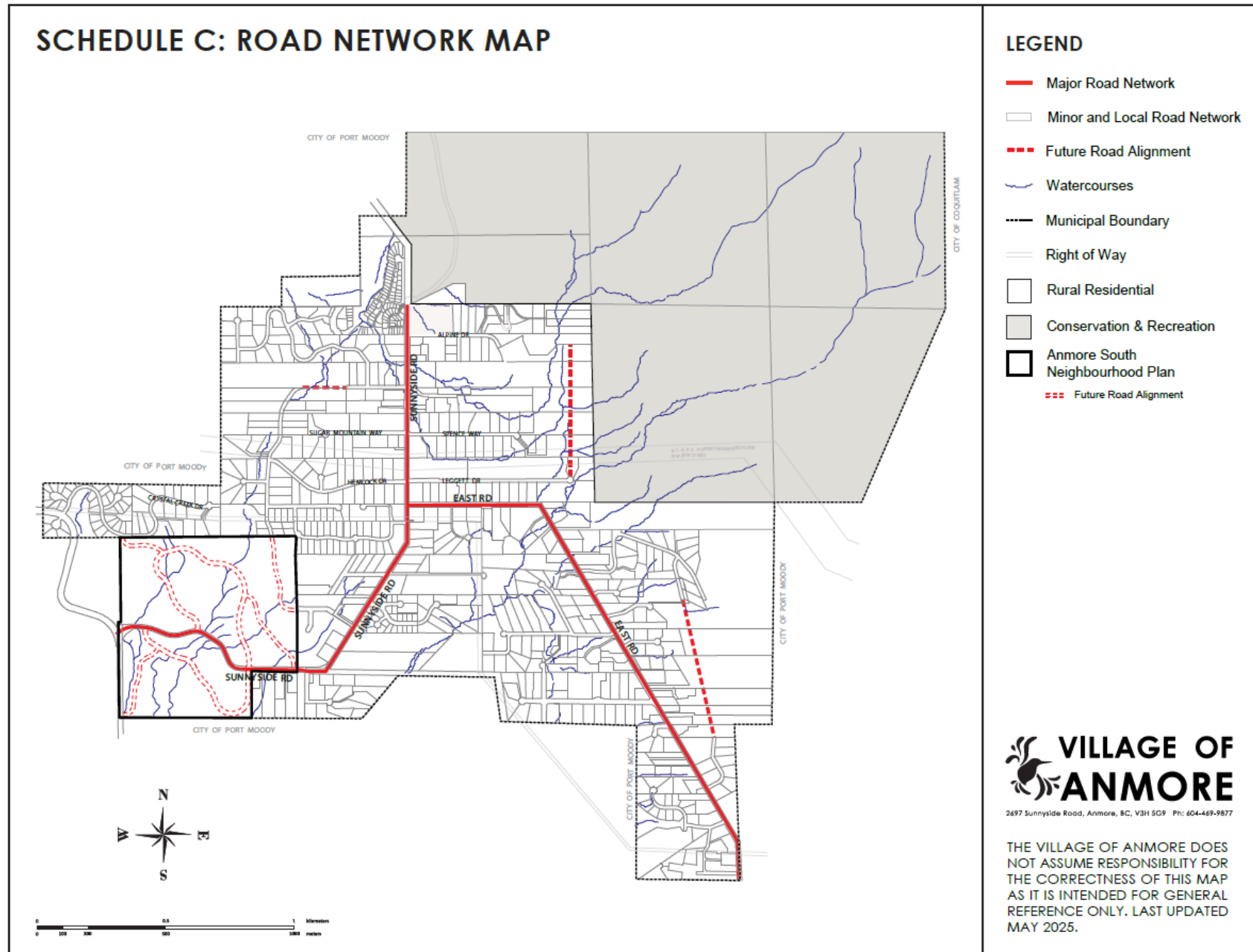
SCHEDULE C – SCHEDULE B1: LAND USE MAP



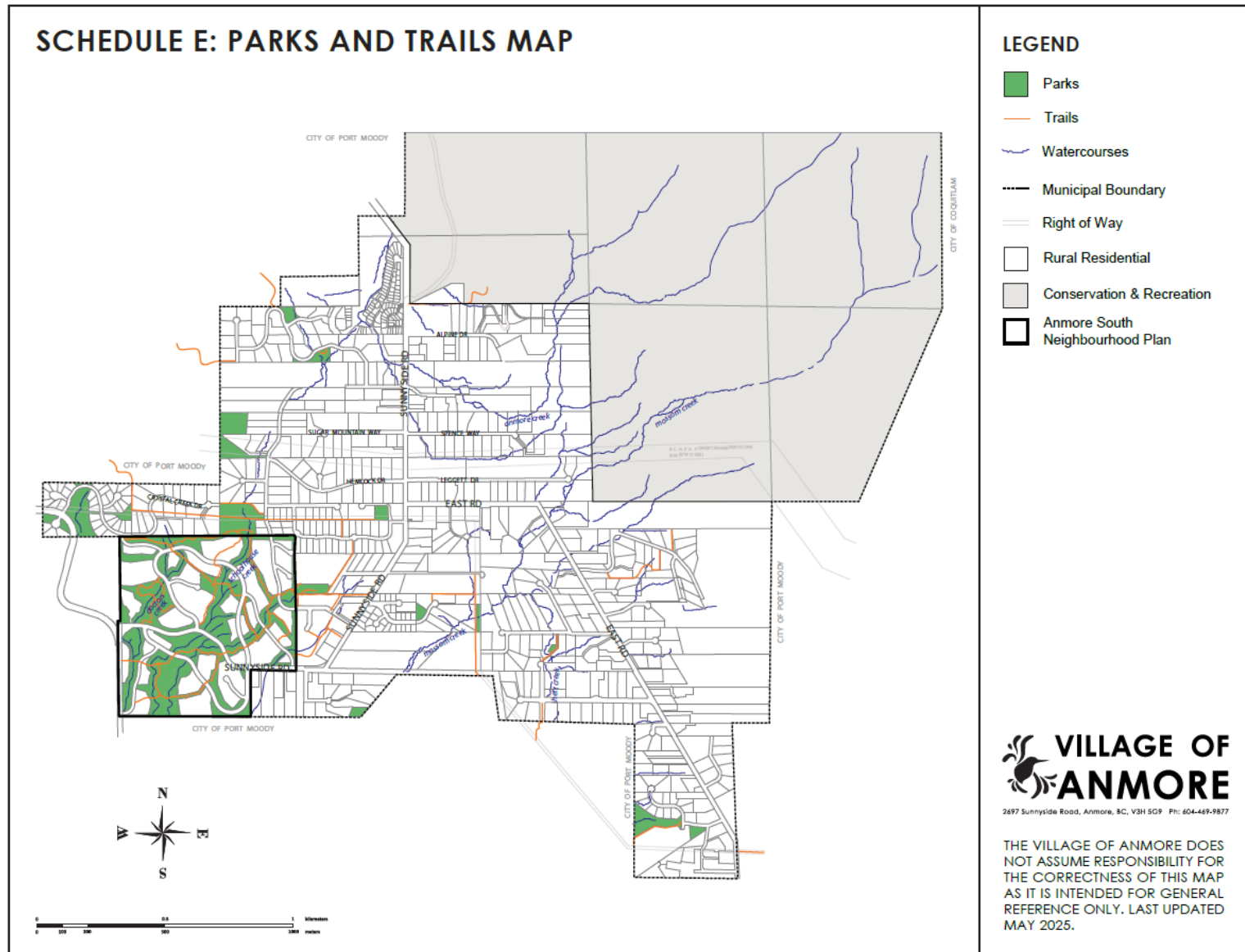
SCHEDULE D – SCHEDULE B2: LAND USE MAP



SCHEDULE E - SCHEDULE C: ROAD NETWORK MAP



SCHEDULE F SCHEDULE E: PARKS AND TRAILS MAP



MEMORANDUM

To: Village of Anmore Council

Subject: Comparative Analysis of Fiscal Impacts: Anmore South Development – 2,202 vs. 1,750 Unit Scenarios

Executive Summary

This memorandum provides an updated fiscal comparison of two build-out scenarios for the Anmore South development: a 2,202-home option and a reduced 1,750-home option. The analysis is based on the Deloitte Fiscal Impact Assessment and reflects updated 2024 budget alignment, service assumptions, and a revised population estimate of 3,911.

Both scenarios yield substantial fiscal surpluses to the Village; however, the 1,750-home scenario provides a notably stronger net return when accounting for police and fire service costs. Specifically, the net fiscal benefit to the Village, inclusive of these costs, is \$611,006 per year under the 1,750-home scenario—slight reduction from the 2,202-home scenario, which had a comparable net of \$696,109.

This analysis demonstrates that a smaller, more strategically planned neighbourhood can deliver similar financial value to the municipality while minimizing infrastructure burden and service escalation.

Fiscal Comparison Summary

Metric	2,205 Homes	1,750 Homes	Difference
Total Revenue	\$9,392,440	\$9,024,413	-\$368,027
Total Costs (incl. Police)	\$5,568,016	\$5,373,935	-\$194,081
Net Fiscal Impact (vs. 2024 Budget)	\$1,297,119	\$1,123,173	-\$173,946
Net Impact (incl. Police + Fire)	\$2,568,999	\$2,483,896	-\$85,103

Key Insights

1. Reduced Population Lowers Service Burden

The updated 1,750-home scenario serves an estimated 3,911 residents—452 fewer than the 2,202-home scenario. This population reduction lowers operational demands on municipal services and infrastructure, producing a \$173,946 annual savings in general government, public works, planning, and policing.

2. Fire Services: Capital and Operational Efficiencies

The annual fire service cost (including amortized capital) is \$1,255,425 under the 2,202-home option and \$1,166,582 under the 1,750-home option. This reflects a savings of \$88,843 annually—driven by fewer required volunteer staff, lower training and equipment demands, and delayed escalation to a full-service operational model.

3. Net Fiscal Advantage with Lower Risk

The 1,750-home option results in a net fiscal benefit (after police and fire service costs) of \$2.48 million—resulting in a reduction of \$85,103 compared to the 2,202-home option. This makes the reduced scenario fiscally superior while also easing planning pressures, such as emergency services scaling and capital asset lifecycle management.

Strategic Considerations

- **Service Thresholds:** Lower population growth delays triggering provincial cost-sharing thresholds for RCMP, reducing long-term policing costs.
- **Infrastructure Sizing:** Despite fewer homes, fixed-cost infrastructure (e.g., reservoirs, sewer) remains necessary and partially offsets unit-level cost efficiencies.
- **Community Amenities:** Reduced density may limit amenity leverage, affecting DCC contributions and scope of public improvements.

Conclusion

The updated fiscal analysis confirms that the 1,750-home scenario has a slight reduction in the net revenue standpoint. The Village would realize an annual positive net impact of \$611,006 after all municipal, police, and fire costs are accounted for, as compared to \$696,109 under the 2,202-home plan. This is a slight reduction per year in net municipal revenue highlights the benefits of a more compact, efficient development footprint.

Comparison 2205 Homes vs. 1750 Homes

TABLE 1 NET FISCAL IMPACT OF ANMORE SOUTH

Revenue & Cost Item	2024 Budget	With 2205 Homes	Net Impact of 2202 Homes	With 1750 Homes	Net Impact of 1705 Homes
TOTAL OF RELEVANT REVENUE	\$5,461,302	\$9,392,440	\$3,931,138	\$9,024,413	\$3,563,111
TOTAL OF RELEVANT COSTS (including Police with Anmore South)	\$2,933,997	\$5,568,016	\$2,634,019	\$5,373,935	\$2,439,938
Net (Village Budget)	\$2,527,305	\$3,824,424	\$1,297,119	\$3,650,478	\$1,123,173
Less Police Service Tax (Paid directly by Anmore taxpayers, is eliminated with Anmore South)	\$313,255	\$0	-\$313,255	\$0	-\$313,255
Less Fire Service Costs (Paid directly by Anmore taxpayers, including operating costs + new capital amortization)	\$341,160	\$1,255,425	\$914,265	\$1,166,582	\$825,422
Net (Including Police + Fire Service Costs)	\$1,872,890	\$2,568,999	\$696,109	\$2,483,896	\$611,006

TABLE 2 SUMMARY OF NET REVENUE IMPACTS FROM ANMORE SOUTH

Revenue Item	2024 Budget	With 2205 Homes	Net Impact of 2202 Homes	With 1750 Homes	Net Impact of 1705 Homes
PROPERTY TAX	\$2,945,439	\$6,583,504	\$3,638,065	\$6,244,624	\$3,299,185
PERMITS, FEES AND CHARGES	\$335,975	\$390,199	\$54,224	\$379,814	\$43,839
GRANTS (including GRANTS-IN-LIEU)	\$1,957,406	\$2,115,977	\$158,571	\$2,113,693	\$156,287
INTEREST & OTHER	\$222,482	\$302,761	\$80,279	\$286,282	\$63,800
TOTAL OF RELEVANT REVENUE	\$5,461,302	\$9,392,440	\$3,931,138	\$9,024,413	\$3,563,111

TABLE 3 SUMMARY OF NET COST IMPACTS FROM ANMORE SOUTH

Cost Item	2024 Budget	With 2202 Homes	Net Impact of 2205 Homes	With 1750 Homes	Net Impact of 1705 Homes
TOTAL GENERAL GOVERNMENT	\$1,863,113	\$2,262,601	\$399,488	\$2,183,820	\$320,707
PUBLIC WORKS (including for new on-site infrastructure)	\$902,491	\$1,197,719	\$295,228	\$1,180,451	\$277,960
PLANNING & DEVELOPMENT	\$168,393	\$215,364	\$46,971	\$205,723	\$37,330
AMORTIZATION OF NEW ASSETS	\$0	\$813,148	\$813,148	\$813,148	\$813,148
POLICE SERVICE COSTS	\$313,255	\$1,079,183	\$765,928	\$990,793	\$677,538
TOTAL RELEVANT COSTS (including current police costs not part of municipal budget)	\$3,247,252	\$5,568,016	\$2,320,764	\$5,373,935	\$2,126,683
Fire Service (Estimated annual operating paid by Anmore residents, including part of current capital budget)	\$341,160	\$1,192,925	\$851,765	\$1,104,082	\$762,922
Annual Lifecycle Costs of New Fire Service Assets (Anmore share)	\$0	\$62,500	\$62,500	\$62,500	\$62,500
TOTAL RELEVANT COSTS (with Fire Service)	\$3,588,412	\$6,823,441	\$3,235,029	\$6,540,517	\$2,952,105